



Notice of meeting of

Planning Committee

- To:** Councillors R Watson (Chair), Bartlett, Blanchard, Cuthbertson, Hill, Horton, Hyman, Jamieson-Ball, Macdonald, Moore, Simpson-Laing, Reid, Smallwood, I Waudby and Wilde
- Date:** Thursday, 27 July 2006
- Time:** 4.30 pm
- Venue:** The Guildhall, York

AGENDA

Please note there are no site visits prior to this meeting.

1. Declarations of Interest

At this point, members are asked to declare any personal or prejudicial interests they may have in the business on this agenda.

2. Minutes

(Pages 1 - 10)

To approve and sign the minutes of the meetings of the Planning Committee held on 27th June 2006 and 6th July 2006.

3. Public Participation

It is at this point in the meeting that members of the public who have registered their wish to speak can do so. The deadline for registering is by 5pm the day before the meeting. Members of the public can speak on specific planning applications or on other agenda items or matters within the remit of the committee.

To register please contact the Democracy Officer for the meeting, on the details at the foot of this agenda.

4. Plans List

This item invites Members to determine the following planning applications:

- a) **Council Depot, Foss Islands Road, York, (Pages 11 - 30)
YO31 7UL (06/00338/GRG3)**
- b) **Council Depot, Foss Islands Road, York, (Pages 31 - 42)
YO31 7UL (06/00333/REMM)**

5. The Racecourse and Terry's Factory (Pages 43 - 98) Conservation Area Character Appraisal - Consultation Draft

This report requests Members to approve a draft consultation character appraisal for the Racecourse and Terry's Factory Conservation Area, for consultation. This document is contained within Annex C.

NOTE:

Annex C to this report has colour maps, photos and plans. Colour copies of this are available as follows:

- Online at www.york.gov.uk (see meetings).
- For inspection upon request at the Guildhall, St Helen's Square, York, YO1 9QN. Please ask for Rowan Hindley.
- A copy is available in the Members Library for Members.

Colour copies will be available at the meeting.

6. Any other business which the Chair considers urgent under the Local Government Act 1972.

For more information about any of the following please contact the Democracy Officer responsible for servicing this meeting on the details below:

Rowan Hindley

Tel 01904 552062 or rowan.hindley@york.gov.uk

- Registering to speak
- Business of the meeting
- Any special arrangements
- Copies of reports

City of York Council

Minutes

MEETING	PLANNING COMMITTEE
DATE	27 JUNE 2006
PRESENT	COUNCILLORS R WATSON (CHAIR), BARTLETT, CUTHBERTSON, HILL, HORTON, HYMAN, JAMIESON-BALL, MACDONALD, REID, SIMPSON-LAING, SMALLWOOD, I WAUDBY, M WAUDBY AND B WATSON
APOLOGIES	COUNCILLORS BLANCHARD, MOORE AND WILDE

1. **DECLARATIONS OF INTEREST**

At this point, members were asked to declare any personal or prejudicial interests they had in the business on this agenda.

Councillor Hyman declared a personal non-prejudicial interest in agenda item 5 'Revised Development Brief for the Terry's Factory site' as he was a council representative on Science City York. He exercised his right to remain in the room and took part in the discussion.

2. **MINUTES**

RESOLVED: That the minutes of the meetings of the Planning Committee, held on 16th May 2006 and 24th May 2006, be approved and signed as a correct record.

3. **PUBLIC PARTICIPATION**

It was reported that there had been no registrations to speak, under the Council's Public Participation Scheme, on general issues within the remit of the Committee.

4. **PLANS LIST**

Members considered a schedule of reports of the Assistant Director (Planning and Sustainable Development), relating to the following planning applications, outlining the proposals and relevant policy considerations and setting out the views and the advice of consultees and officers.

4.a **Royal Masonic Benevolent Institute Homes, Connaught Court, St Oswald's Road (05/02546/OUT)**

Members considered an outline application, submitted by the Royal Masonic Benevolent Institution, for the erection of sheltered accommodation, extension to the Elderly Mentally Frail unit, residential development, relocation of the existing bowling green and provision of a new access road and car parking (revised scheme). (Ref: 05/0022/OUT)

Officers updated that an error had been made in informing neighbours about the site visit and committee date, and advised Members that it would not be appropriate to consider the application. Apologies were expressed to all concerned that the correct procedures had not been followed.

Officers also updated that the Applicant had submitted revised proposals and had asked for the plan to be substituted. Officers from Planning and Legal Services advised Members that it would be appropriate to defer the application, in order to allow for consultation on the revised proposal and for Officers to consider the changes and revise the report.

Some Members raised concerns that the Applicant had submitted revised proposals at a late stage before the meeting as this was unfair to objectors.

Members requested that it be noted that they raised concerns that the Applicant had submitted revised proposals at such a late stage. Members requested that a letter be written to the Agent for the Applicant requesting an explanation of their actions.

Members of the public who had registered to speak on the item all agreed to defer their right to speak until the application was considered again by the committee.

- RESOLVED:
- (i) That the application be deferred to a future meeting.
 - (ii) That a letter be written to the Agent for the Applicant requesting an explanation for the submission of revised proposals, at such a late stage before the meeting.

REASON: To ensure full consultation on the revisions submitted by the Applicant.

4.b Site Covered By Properties 1 To 7 And 15 To 22, Bleachfield, Heslington (06/00826/FULM)

Members considered a full major application, submitted by University of York, for demolition of university staff houses and erection of six student residences, comprising of 3 x three storey and 3 x four storey blocks with an associated utility building, parking and landscaping (revised scheme). (Ref: 06/00826/FULM)

Officers updated that an error had been made in informing neighbours about the site visit and committee date, and advised Members that it would not be appropriate to consider the application. Apologies were expressed to all concerned that the correct procedures had not been followed.

RESOLVED: That the application be deferred to a future meeting.

REASON: To ensure the correct procedures are followed.

5. REVISED DEVELOPMENT BRIEF FOR THE TERRY'S FACTORY SITE

This report described the consultation process carried out on the draft development brief for the Terry's Factory Site. It presented a revised brief which had been compiled in response to the concerns and suggestions raised, and sought its approval as draft supplementary planning guidance to the Development Control Local Plan.

Angela Michel and Paul Robinson spoke as representatives of The Stables Project, on the community based proposals.

Angela Michel spoke about the educational project which catered for 14 pupils who were outside mainstream education for a wide range of reasons. The project worked with pupils on their individual development. There was local community involvement, and the work space could be hired. The project had applied for funding from the Arts Council as the project did not have enough work space.

Paul Robinson outlined how the project met the objectives of the Brief and provided arts and cultural activities for adults and children. If the project was on the site there would be a useful educational link with Science City York, in terms of IT and industry. The project had been recognised by a range of council departments, and would provide jobs. The proposal included a café which would be the central focus of the learning centre.

Officers reported that the scheme was welcomed in terms of the development brief, and Officers could potentially facilitate dialogue with the developer.

Members discussed the following:

- The percentage of the site to be dedicated to Science City York had not been included. The emphasis was to be on Science City York, but the final percentage depended on the master planning process. At this stage Officers did not want to be prescriptive.
- Members raised that residents had made representation against a hotel venue on the site. Officer reported that there had been a mixed response to this proposal and the Tourism Bureau had set out evidence of a need for this.
- There was a desire for Knavesmire Primary School to gain a sports site. It was reported that on pg124 of the agenda, a paragraph had been added in respect of this, and would be discussed with the developer.
- It was raised that removal of the sentence 'The involvement of the local community in identifying needs is therefore important' on pg123, paragraph 8.2, weakened community involvement. It was reported that Officers were in a position to encourage the developer but not stipulate terms of the development.
- Members raised that there were a large number of comments relating to transport, and traffic on Bishopthorpe Road, yet Officers had not suggested changes in light of these comments. It was

reported that these issues could not be addressed as it was not known exactly what the issues would be. A Traffic Assessment would be carried out in due course and Officers would then consider traffic related comments.

- It was requested that Officers check that the fountain referred to on pg 134 of the consultation responses (Background paper), did in fact have no heritage value.
- The Brief mentioned that any trees that did not add quality to site would be removed. Concerns were raised why all trees could not be kept as the site was large and this would help to retain its character.
- Long bullet-pointed lists should be referenced with numbering.
- On pg119, bullet point 11, it was queried what the term 'legible' meant in that context. Officers reported that it was a design term used to describe making something understandable to the public. It was agreed that this term should be explained.
- On pg101, at 4.2, concerns were raised that use of the car park should be related to the site, and the brief should specify this.
- Information regarding the Public Art Strategy 1% for art policy should be brought forward into the Brief.
- It was requested that on pg125, at 8.9, the word 'encouraged' be changed to 'required', to ensure involvement of Knavesmire Primary School.
- It was requested that the section on bus services on pg 129, at 9.13, be strengthened, so that the financial assistance for bus services should continue until the development was fully occupied.
- Members requested that there should be a requirement for any discount sale affordable housing units that remain unsold, to be made available for affordable rent.

Members requested that their thanks be recorded to all those who had submitted comments.

RESOLVED: That the attached revised Development Brief be approved, with the above amendments, as non-statutory draft supplementary planning guidance to the City of York Development Control Local Plan, as a basis for negotiating an appropriate scheme to redevelop the site and for considering planning and listed building / conservation area consent applications.

REASON: 1. The redevelopment of the site is an important opportunity to provide quality accommodation for a range of uses that will support the York economy and a Development Brief is considered the most appropriate approach for the Council to set out a vision, objectives and clear guidance for a new sustainable employment led mixed use development to create a community of complementary uses.

2. The conservation importance and prominent setting of the site require detailed consideration and a Development Brief is considered the most appropriate approach for the Council to set out the key considerations for the site and requirements of potential developers.

6. CHAIR'S REMARKS

The Chair informed Members that an additional Planning Committee meeting had been arranged to consider the application for Bleachfield. Details of the site visit and meeting were as follows:

Site Visit: Wednesday 5th July 2006, 1.30pm (meet at Memorial Gardens)

Committee: Thursday 6th July 2006, 4.30pm, Council Chamber

COUNCILLOR R WATSON
Chair

The meeting started at 4.30 pm and finished at 5.55 pm.

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MEETING	PLANNING COMMITTEE
DATE	6 JULY 2006
PRESENT	COUNCILLORS R WATSON (CHAIR), CUTHBERTSON, HORTON, JAMIESON-BALL, LIVESLEY, MACDONALD, MOORE, REID, SIMPSON-LAING, I WAUDBY, M WAUDBY AND B WATSON
APOLOGIES	COUNCILLORS BARTLETT, BLANCHARD, HILL, HYMAN, SMALLWOOD AND WILDE

7. DECLARATIONS OF INTEREST

At this point Members were asked to declare any personal or prejudicial interests they had in the business on this agenda. No interests were declared.

8. PUBLIC PARTICIPATION

It was reported that there were no registrations to speak under the Council's Public Participation Scheme on agenda items or items within the remit of the committee.

9. PLANS LIST

Members considered a report of the Assistant Director (Planning and Sustainable Development), relating to the following planning application, outlining the proposals and relevant policy considerations and setting out the views and the advice of consultees and officers.

a. **Site Covered By Properties 1 To 7 And 15 To 22, Bleachfield, Heslington (06/00826/FULM)**

Members considered an full application, submitted by University of York, for the demolition of university staff houses and erection of six student residences, comprising 3 x three storey and 3 x four storey blocks with associated utility building, parking and landscaping (revised scheme). (Ref: 06/00826/FULM).

An update was provided by Officers as follows:

- The site address should be '2-22 Bleachfield', as according to the site boundary defined on the submitted plans.
- Condition 7 – insert requirement 'to implement in accordance with the approved method statement'.
- Conditions 11-14 should be amended to a single condition as follows:

11. A site investigation shall be undertaken based upon the findings of the desk study.
 - a. The investigation shall be carried out in accordance with BS10175: Investigation of potentially contaminated land: code of practice. The results of the investigation shall be submitted to and approved by the local planning authority in writing prior to any development commencing on the site.
 - b. A risk-based remedial strategy (which shall have due regard for UK adopted policy on risk assessment and shall be developed in full consultation with the appropriate regulator(s)) shall be developed based on the findings of the site investigation. The remedial strategy shall be submitted to and approved by the local planning authority in writing. The approved strategy shall be fully implemented prior to any development commencing on site.
 - c. A validation report shall be submitted to and approved by the local planning authority, detailing sample locations and contaminant concentrations prior to any development commencing on site.
 - d. Any contamination detected during site works that has not been considered within the remedial strategy shall be reported to the local planning authority. Any remediation for this contamination shall be agreed with the local planning authority and fully implemented prior to any further development of the site.
 - e. A timetable of proposed remedial works shall be submitted to the local planning authority prior to any works being undertaken on site.

Reason: To protect the health and safety of workers on site and residents of the proposed dwellings, and to ensure the integrity of construction materials.

- Conditions 15-27 – following the deletion of conditions 12, 13 and 14, conditions 15 onwards will need renumbering.
- Condition 27 (as numbered in the report) – the last words of this conditions should read ‘by the local planning authority’.
- It was reported that a further letter of objection had been received from a resident of Heworth Green. Copies were circulated.

Representation were made in objection to the proposal on the grounds of loss of amenity, loss of design quality and lack of sustainability. Representations were made in support by the Agent for the Applicant.

Members clarified that there were recycling facilities and these were located in the communal kitchens. Such provision was University policy. Members queried about accommodation for disabled students, and requested a condition, if the application was approved, to ensure there was a parking space for each disabled students who could be accommodated on the site. It was requested that Officers slightly reword condition 7, so the list of prohibited activities in paragraph three was clear. Members requested a condition, if the application was approved, to ensure a travel plan was in place for students arriving and leaving the site at each end of the terms.

Some Members supported the application and the increase in on-site good quality university accommodation.

Some Members raised concerns about design of the buildings and context. Some Members also raised concerns about disabled access to the buildings and the lost family accommodation.

RESOLVED: That the application be refused.

REASON:

In its simplistic block layout and massing, and repetitive, unrefined architectural design, the scheme fails to achieve the standards required by Policy ED6 of the Draft Development Control Local Plan (incorporating the 4th set of changes) which states that new development should be of a high standard of design appropriate to the setting of the University. In particular the scheme fails to respond to the special landscape character of the site which is intrinsic to the identity of the University Campus, and thereby does not meet criteria set out in the Council's Development Brief for the Heslington Campus. In addition the unsympathetic site planning and design are inappropriate to the 'gateway' context of the site, such that overall the proposal is in conflict with PPS 1 (Delivering Sustainable Development) which states that design which is inappropriate to context and fails to take opportunities available for improving the character and quality of an area should not be accepted.

COUNCILLOR R WATSON
Chair

The meeting started at 4.40 pm and finished at 5.55 pm.

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- Non-food retail units with a footprint of 6,970 sq m (75,000 sq ft) and gross floorspace of 10,220 sq m (110,00 sq ft)
- A foodstore of 6,718 sq m (82,000 sq ft) for occupation by Morrison Supermarkets, to include a petrol filling station.
- A restaurant of 372 sq m (4,000 sq ft)
- Relocation of Kwik Fit 643 sq m (6,830 sq ft).

1.6 That outline permission was then amended by planning permission 04/04420/GRG4 dated 24th February 2004. This was also a Section 73 application and was sought in order to allow the redevelopment of the site in phases and introduced a phasing condition to allow for this. A number of the other conditions were also amended. A further outline permission was then issued on 2nd September 2005 which further varied condition 4 (non-food sales floorspace be limited) of 04/04420/GRG4

1.7 A reserved matters application was submitted in December 2004 illustrating the same layout and footprint as that indicated at the time of the outline application, but also including details of the cycle, bus and pedestrian route onto and through the site from Foss Islands Road to the James Street Link Road. This is the only reserved matters approval granted so far.

2.0 POLICY CONTEXT

2.1 Development Plan Allocation:

Floodzone 3 Flood Zone 3

Floodzone 2 Flood Zone 2 CONF

DC Area Teams Multiple (Spatial)

City Boundary York City Boundary 0001

Conservation Area Central Historic Core 0038

Areas of Archaeological Interest City Centre Area 0006

Listed Building - Grade II

2.2 Policies:

CYS1

Land allocated for shopping sites

CYS2

Out of centre retail warehouse criteria

CYSP7

The sequential approach to development

CYSP8

Reducing dependence on the car

CYHE4

Listed Buildings

CYNE7

Habitat protection and creation

CYNE8

Green corridors

3.0 CONSULTATIONS

3.1 INTERNAL.

3.2 City Development.

No policy issues. Alteration to the condition restricting the net sales area selling sports goods tightens up the restriction that no more than 50% of the net sales area in ANY unit selling sports goods shall be used for the sale or display of clothing or footwear (previous condition was for 50% for the total of all units). This is a minor change and has no policy implications.

Alteration to the condition restricting the amount of sales floorspace used for the sale and display of the items restricted for sale in the non-food retail units. It is now proposed to read that this level shall not exceed 15% in ANY unit as opposed to the previous condition was for 15% for the total of all units. This is considered a minor change to clarify that the restriction will be applied on a unit by unit basis. No policy implications.

3.3 Highway network management.

No objections.

EXTERNAL.

3.4 Environment Agency.

Environment Agency understand that conditions 18,23, 24, 25 and 26 shall be carried over to the new application. On this basis have no further comments to make.

3.5 Yorkshire Water.

Comments as previous.

3.6 Foss Internal Drainage Board.

Has no objections to the proposals, however the planning and drainage authorities are reminded that no works undertaken by the developer of the site should be allowed to have a detrimental effect on water levels on the River Foss or the Osbaldwick/Tang Hall Beck.

3.7 Heworth Planning Panel.

No objections.

3.8 York Cycle Campaign.

Object. Plans do not make the most of the opportunity to link up the Foss Islands cycle route with a possible link into the city centre, possibly through the new Hungate development. The development could be excellently served by high quality cycle routes which provide a much needed traffic free access into the city centre from the residential areas further out such as Heworth, Tang Hall and Osbaldwick. The plans presented do not make the most of this opportunity and the cycle facilities provided are badly designed and unattractive.

- Development has been designed as a car based development.
- impact on traffic generation
- wider effects on local shops in the York area.

- non-cyclists often won't cycle because of volume of traffic and this development can only increase this.
- cycle and pedestrian facilities give the impression of not being very well thought through - they are indirect and discontinuous.
- unclear where / how cyclists will get on and off the route.
- unclear how they will get onto the future link road.
- unclear how cyclists will access the proposed cycleway along the Foss or from the new Hungate development. This appears to have been ignored but could be a huge benefit to the city.
- unclear how cyclists access the cycle parking for the supermarket via the cycle route.
- proposed green corridor is not sufficiently wide to provide a continuous, fully viable 'green corridor' or a pleasant environment for cyclists or pedestrians. Understand that this was originally agreed upon by the planning committee.

3.9 York Natural Environment Trust.

States that the way these applications have been presented, that is simply as minor amendments intended to consolidate conditions and plans for the Morrisons scheme to be an attempt to obtain by stealth, planning consents for what amounts to a new scheme. It should be subject to a new outline application which only if approved should then require a new reserved matters application. Applications should be subject to a full public consultation in a form which makes their true intent clear and should ultimately be referred to the Sec of State for approval.

- plans lack clarity and fail to provide a clear key. Scale is misleading.
- drainage plans are unsustainable.
- width of the tree belt alongside Foss Islands Road specified in the original application as 10 metres now averages only 7 metres and falls as low as 5 in places. This is insufficient to provide the depth of ameliorating and screening originally specified and will be inadequate for its purpose.
- the green corridor is inadequate. In both the previous and this application it appears as only a transport corridor with some disjointed elements of embellished landscaping provided.
- as a route for people, the footway / cycleway is disjointed and made hazardous by being mixed up with motor vehicle roads. Will not be a pleasant experience for people using it as a link between the river foss corridor and St. Nicholas fields.
- During consideration of the 'Morrisons' scheme, it became clear that the failure of the green corridor concept had resulted from failure to resolve at the outline stage, conflicts between space needed for landscape, and habitat provision, space needed for a safe and pleasant pedestrian/cycle route, space required for a bus route and space aspired to by developers to maximise car parking. It is essential in the new 'Morrisons/Homebase' scheme that these conflicts are resolved and provision made for a continuous, fully viable green and wildlife corridor, accommodating a safe and pleasant pedestrian/cycle route, segregated from motor traffic, before the outline application is determined.

4.0 APPRAISAL

4.1 Section 73 of the 1990 Town and Country Planning Act provides for applications for planning permission without complying with conditions previously imposed on a planning permission. The Local Planning Authority can grant such permissions unconditionally or subject to different conditions, or they can refuse the application if they decide the original conditions should continue in the same form or that any changes will be materially harmful or have policy connotations. The proposal here is to vary some of the original conditions and if approved, a completely new decision notice would be issued.

4.2 The key issue here is whether the proposed variation of the conditions allows the Council to retain the necessary control over the development in terms of details and timescale. The precision and enforceability of any reworded conditions is also a key consideration.

4.3 As outlined in 1.2 above, the application site has three extant outline planning permissions and one reserved matters permission. This application deals with policy issues that arise from the alterations, rather than the detailed design and layout issues which are dealt with as an amendment to the reserved matters permission which is the next agenda item.

4.4 The three outline permissions (original outline decision and two section 73 amendments) mean that it is somewhat confusing when looking at the site history and so the purpose of this amendment is both to introduce a number of amendments to the scheme details and to consolidate the three permissions into one. For the purposes of the application, it is still assumed that the development will continue in a phased manner and that revised conditions will reflect the Section 73 amendment (04/004420/GRG4) approved 7th June 2005.

4.5 It is a material consideration that permission has already been granted for this scheme and unless there has been a material change in circumstance, then the principal of the scheme cannot be revisited here. The extent of the changes are relatively minor and mainly relate to the minor layout and design changes which are dealt with on the amendment to the reserved matters permission next up on this agenda.

4.6 However, there are alterations requested to the conditions on the outline approval, from which the reserved matters will be based. The main changes are directly related to the agreement reached that Homebase are to operate from one of the units on the non-food retail park. This has led to an alteration in the footprint of the building comprising the non-food retail units because, given their trading requirements, they require a larger unit and the inclusion of a garden centre. The key issue here therefore is to ensure that these changes are in line with the agreed details and that the retail floorspace restrictions already in place are not compromised.

4.7 The site is allocated in the draft local plan for retail use and historically it has been agreed that the non-food retail units will have a footprint of 6970sqm (although there is no conditioned limit on this) and a gross floorspace of 10220sqm (this allowed for mezzanines). It is not the intention of the applicant to seek any alteration to this agreed floorspace area although the footprint of the garden centre shown on the revised site layout accompanying the revised reserved matters application confirms that the footprint of the now 4 units is 6967sqm's (plus the 706sqm garden centre) encompassing a gross floorspace of 8990sq.m. Control on the floorspace area is the important factor here as it is this that potentially impacts on the shops in the City centre. The footprint area over which this is contained is not so important providing this does not impact on levels of car parking within the site and local visual amenity, neither of which the revised footprint does. There are no confirmed occupiers for the other 3 units as yet and therefore they will, if they so wish, make up the floorspace shortfall (1230sqm still spare) on a first come first serve basis by the use of mezzanine flooring. Condition 12 is recommended to be revised to ensure that this is understood and to ensure that the 10220 sqm floorspace area actually appears in a condition for the first time.

4.8 The significant alteration shown on the revised proposed layout is the introduction of a Homebase run garden centre (706sq.m) attached to the southern elevation of the main Homebase Unit and this was submitted to be shown to be in addition to the restricted floorspace area of 10220sqm. However officers consider that this would constitute a potentially significant change that could not be dealt with as part of a Section 73 application and that introducing such a change would require a revised retail assessment to be carried

out. Whilst there are no objections to a garden centre on the site (it is classed as a 'bulky good' which will not impact on the City Centre shops and is not one of the restricted items that cannot be sold from this site; see condition 8), the Council do expect this garden centre floorspace to be included in the 10220sqm floorspace restriction and therefore future occupiers of the unoccupied units simply have less space available. The applicant's agent has agreed this.

4.9 Amendments are also sought to the conditions restricting a) the net sales area selling sports goods and b) restricting the amount of sales floorspace in the non-food units set aside for those goods outlined in condition 8. In both cases the changes tighten up on this restriction and are a minor change which have no policy implications.

4.10 The amendment to condition 23 is considered acceptable and is a minor change which officers consider does not effect future control or local amenity. The details required now specifically relate to the restaurant only rather than across the entire site where no such extract systems are required. Separate environmental legislation now also exists to control any harm that arises from these.

4.11 The comments of the York Natural Environmental Trust and the York Cycle Campaign are noted and acknowledged. However, the scheme does already have planning approval and the Council cannot now revisit these details. Even if members refused these changes, the existing planning permission, including layout can still be implemented in full. Given that there are no policy implications of the changes and the layout changes are minor, officers do not consider that full revised applications are necessary.

5.0 CONCLUSION

5.1 The alterations proposed do not appear to present any significant issues from a planning point of view given that neither the approved footprint nor the gross floorspace already agreed is to be changed. There are no changes in the permitted uses proposed and as such officers have concluded that the changes do not require the retail or transport assessments carried out and agreed to facilitate the determination of the original outline permission to be revisited. The changes in wording to the conditions are minor and in some cases actually tighten control rather than loosen it.

5.2 Subject to:-

- a) The Secretary of State allowing the Authority to determine this application
- b) Amendment of the S. 106 Agreement to refer to this variation application
- c) Imposition of the conditions as listed below (the changes to wording are highlighted in bold for members information and ease of reference).

it is recommended that the application be approved.

6.0 RECOMMENDATION: Approve subject to Section 106 and SOS

- 1 OUT1 Approval of Reserved Matters
- 2 A phasing programme shall be submitted to and agreed in writing by the local planning authority and the development shall be implemented in accordance with the agreed phasing programme.

Reason: To ensure that the Local Planning Authority is satisfied with the details of the phasing of the development.

3 OUT2 Full details to be submitted

4 The development shall not be carried out otherwise than in complete accordance with the plans as originally submitted and later amended by the revised drawings listed below and received on the date indicated, or as may otherwise be agreed in writing with the local planning authority.

Flood risk assessment received 9th February 2006

Dwg No. 221 B (Harris) Simplified site layout received 9th February 2006

Dwg No. H/06 P1 (White Young Green) Foss Islands Road junction details received 9th February 2006.

Reason: To achieve an acceptable form of development.

5 Note: Conditions 4 to 6 relate to the food retail element of the scheme;

The amount of non-food sales floorspace should be limited to no more than 15% of the net floorspace granted permission for construction of the original foodstore.
n.b. the current scheme proposes 3,721 sqm (net) with 558 sqm of non-food goods.
This equates to 15%

Reason: To minimise the impact of comparison goods sold at the store competing with York City Centre retail outlets selling the same goods.

6 The installation of a mezzanine floor other than for ancillary offices, staff facilities and storage, shall not be permitted unless otherwise agreed in writing by the Local Planning Authority.

Reason: To avoid the proposed development having an adverse impact on York City Centre.

7 The net retail floorspace shall not exceed 3,721 square metres without prior written approval from the Local Planning Authority.

Reason: To avoid the proposed development having an adverse impact on York City Centre.

8 Note: Conditions 7 to 11 relate to the non-food retail element of the scheme:

The following range of goods shall not be sold unless ancillary to the main range of goods:

- Clothing and Footwear
- Accessories
- Watches and Jewellery
- Music and Video recordings and Video and CD Rom
- Camera and Photographic equipment
- TV/Video/DVD and Hi-Fi equipment
- Toys
- Pharmaceutical goods
- Books/Magazines
- Leisure and Luxury goods (e.g. handbags, luggage, china, glass goods and cutlery)

- Any use falling within Class A2 of the Use Classes Order

Reason: To minimise the impact of comparison goods sold at these stores competing with York City Centre retail outlets selling the same goods.

- 9 No more than 50% of the net sales area in **any** unit selling sports goods shall be used for the sale or display of clothing or footwear.

Reason: To minimise the impact of comparison goods sold at these stores competing with York City Centre retail outlets selling the same goods.

NOTE: This replaces the previous condition which said ' No more than 50% of the net sales area in units selling sports goods, shall be used for the sale or display of clothing and footwear'.

- 10 The amount of sales floorspace used for the sale and display of those goods restricted in condition 7 **in any unit will not exceed 15% of the total net floorspace within that unit.**

Reason: To minimise the impact of comparison goods sold at these stores competing with York City Centre retail outlets selling the same goods.

NOTE. This replaces the previous condition which said. 'The amount of sales floorspace used for the sale and display of those goods restricted in Condition 7 will not exceed 15% of the total net floorspace'.

- 11 No unit shall be less than 929 sq m net and shall not subsequently be subdivided without the prior written consent of the Local Planning Authority.

Reason: To ensure that smaller units do not trade from the site which could have a detrimental impact on the vitality and viability of York City Centre.

- 12 Additional mezzanine floors (beyond those explicitly referred to in the original planning permission **allowing a total floorspace for the non-food units of 10220sqm excluding, for the avoidance of doubt, the foodstore, the restaurant and motorist repair centre** will not be permitted unless agreed in writing with the Local Planning Authority. **The garden centre shown on the southern elevation of unit 4 is considered part of the floorspace of unit 4 and shall not be occupied as a separate unit.**

Reason: To avoid the proposed development having an adverse impact on York City Centre.

- 13 No development shall take place until full details of a habitat relocation/creation scheme relating to the swamp area and to the established grassland components of the site have been submitted and approved in writing by the local planning authority. The details will include:
- i) The purpose and aim of the relocation proposals
 - ii) A review of the sites potential and any constraints exhibited on the site.
 - iii) Description of the target habitats and species profiles for the habitats to be relocated.
 - iv) The techniques and method statements to be employed by the site preparation and translocation process.
 - v) Timing of the works

- vi) Monitoring
- vi) Proposals for the aftercare and long term management of the translocated site.

All works are to be implemented as approved unless otherwise authorised in writing by the planning authority. The translocation work shall be carried out prior to the implementation of any other works that will affect the source site.

Reason: In order to mitigate against the loss of the swamp and grassland components.

- 14 No development shall take place until there has been submitted and approved in writing by the Local Planning Authority a detailed landscaping scheme which takes account of the loss of existing habitats, which mitigates against the losses incurred and which ensures the provision of a green corridor through the site. The landscaping scheme shall illustrate the number, species, height and position of trees and shrubs to be removed and to be planted. This scheme shall be implemented within a period of six months of the completion of the development. Any trees or plants which within a period of five years from the completion of the development die, are removed or become seriously damaged or diseased shall be replaced in the next planting season with others of a similar size and species, unless alternatives are agreed in writing by the Local Planning Authority.

Reason: So that the Local Planning Authority may be satisfied with the variety, suitability and disposition of species within the site.

- 15 No work shall commence on site until the applicant has secured the implementation of a programme of archaeological work (a watching brief on all ground works by an approved archaeological unit) in accordance with a specification supplied by the Local Planning Authority. This programme and the archaeological unit shall be approved in writing by the Local Planning Authority before development commences.

Reason: The site lies within an Area of Archaeological Importance and the development will affect important archaeological deposits which must be recorded during the construction programme.

- 16 Details of all means of enclosure to the site boundaries shall be submitted to and approved in writing by the Local Planning Authority before the development commences and shall be provided before the development is occupied.

Reason: In the interests of the visual amenities of the area.

- 17 The permission hereby granted for the use of the restaurant for the purpose contained within Class A3 (food and drink) of Part A of the Schedule of the Town and Country Planning (Use Classes) Order 1987, specifically excludes their use for the sale of hot food or drink for consumption off the premises.

Reason: The use of the restaurant for the sale of hot food or drink for consumption off the premises is likely to be detrimental to the amenities of nearby residents and/or detrimental to highway safety.

- 18 Prior to the commencement of any work on site, a detailed method of works statement shall be submitted to and agreed in writing by the Local Planning Authority. This statement shall include the precautions to be taken to ensure that noise does not cause a loss of amenity for neighbouring residents. In particular the statement shall include the hours during which demolition, construction and deliveries to and

from the site will be permitted. The approved method of works statement shall thereafter be complied with at all times, unless a variation of the statement is first agreed in writing by the Local Planning Authority.

Reason: To safeguard the amenities of local residents.

- 19 Prior to commencement of the development hereby permitted, the method for any piling operations shall be agreed in writing by the local planning authority. All piling operations shall be done in accordance with this agreement.

Reason: To safeguard the amenities of local residents

- 20 Prior to the commencement of any work on site, a detailed scheme for controlling the noise of deliveries to and from the site shall be agreed in writing by the Local Planning Authority. This scheme shall include measures to control noise from reversing vehicles and noise from the unloading/loading of vehicles. The hours during which deliveries will be permitted shall also be included. The approved scheme shall thereafter be complied with at all times, unless a variation of the scheme is first agreed in writing by the Local Planning Authority.

Reason: To safeguard the amenities of local residents

- 21 Details of the proposed hours of use for the development shall be submitted to the local planning authority for written approval. The use hereby permitted shall be confined to the hours approved.

Reason: To safeguard the amenities of local residents

- 22 Details of all machinery, plant and equipment to be installed in or located on the use hereby permitted, which is audible outside of the site boundary when in use, shall be submitted to the local planning authority for approval. These details shall include maximum (L_{Amax}(f)) and average sound levels (L_{Aeq}), octave band noise levels and any proposed noise mitigation measures. All such approved machinery, plant and equipment shall not be used on the site except in accordance with the prior written approval of the local planning authority. The machinery, plant or equipment and any approved noise mitigation measures shall be fully implemented and operational before the proposed use first opens and shall be appropriately maintained thereafter.

Reason: To safeguard the amenities of local residents

- 23 **In relation to the restaurant use hereby permitted** there shall be adequate facilities for the treatment and extraction of fumes so that there is no adverse impact on the amenities of local residents by reason of fumes, odour or noise. Details of the extraction plant or machinery and any filtration system required shall be submitted to the local planning authority for approval. Once approved it shall be installed and fully operational before the proposed use first opens and shall be appropriately maintained thereafter.

Reason: To safeguard the amenities of local residents.

NOTE. The previous condition read as follows:

There shall be adequate facilities for the treatment and extraction of fumes so that there is no adverse impact on the amenities of local residents by reason of fumes, odour or noise. Details of the extraction plant or machinery and any filtration system required shall be submitted to the local planning authority for approval. Once

approved it shall be installed and fully operational before the proposed use first opens and shall be appropriately maintained thereafter.

- 24 A site investigation shall be undertaken based upon the findings of the desk study. The investigation shall be carried out in accordance with BS10175: Investigation of potentially contaminated land: code of practice. The results of the investigation shall be submitted to and approved by the local planning authority in writing prior to any development commencing on the site.

Reason: In the interests of the safe and proper development of the site.

- 25 A risk-based remedial strategy shall be developed based on the findings of the site investigation. The remedial strategy shall be submitted to and approved by the local planning authority in writing. The approved strategy shall be fully implemented prior to any development commencing on site.

Reason: In the interests of the safe and proper development of the site.

- 26 A validation report shall be submitted to and approved by the local planning authority, detailing sample locations and contaminant concentrations prior to any development commencing on site.

Reason: In the interests of the safe and proper development of the site.

- 27 Any contamination detected during site works that has not been considered within the remedial strategy shall be reported to the local planning authority. Any remediation for this contamination shall be agreed with the local planning authority and fully implemented prior to any further development of the site.

Reason: In the interests of the safe and proper development of the site.

- 28 A timetable of proposed remedial works shall be submitted to the local planning authority prior to any works being undertaken on the site.

Reason: In the interests of the safe and proper development of the site.

- 29 A method of sampling and validation shall be produced to ensure imported and excavated materials used on site are not contaminated. This should include, where possible, details of the origin of such materials. The methodology shall be submitted in writing and approved by the local planning authority prior to commencement of the development. The approved methodology shall be fully implemented during the construction of the development hereby approved.

Reason: In the interests of the safe and proper development of the site.

- 30 Floor levels shall be set at least to the minimum agreed level of 10.30 metres above Ordnance Datum.

Reason: To protect the development from flooding.

- 31 No development approved by this permission shall be commenced until:

a) A site investigation has been designed for the site using the information obtained from the desk top study and any diagrammatical representations (Conceptual Model). This should be submitted to, and approved in writing by the LPA prior to that

investigation being carried out on the site. The investigation must be comprehensive enough to enable:

- a risk assessment to be undertaken relating to ground and surface waters associated on and off the site that may be affected, and
- refinement of the Conceptual Model, and
- the development of a Method Statement detailing the remediation requirements

b) The site investigation has been undertaken in accordance with details approved by the LPA and a risk assessment has been undertaken.

c) A method Statement detailing the remediation requirements, including measures to minimise the impact on ground and surface waters, using the information obtained from the Site Investigation has been submitted to the LPA. This should be approved in writing by the LPA prior to that remediation being carried out on the site.

Reason: To ensure that the proposed site investigations and remediation will not cause pollution of Controlled Waters.

- 32 No development approved by this permission shall be commenced until a scheme for the provision and implementation of a surface water run-off limitation to Greenfield run-off rates has been submitted to and approved in writing by of the Local Planning Authority. The scheme shall be implemented in accordance with the approved programme and details.

Reason: To prevent the increased risk of flooding.

- 33 No development approved by this permission shall be commenced until a scheme for the provision of surface water drainage works including details of any balancing works and off-site works has been submitted to and approved in writing by the Local Planning Authority. The drainage works shall be completed in accordance with the details and timetable agreed.

Reason: To prevent the increased risk of flooding by ensuring the provision of a satisfactory means of surface water disposal.

- 34 The site shall be developed with separate systems of drainage for foul and surface water on and off site.

Reason: In the interests of satisfactory and sustainable drainage.

- 35 Unless otherwise approved in writing by the Local Planning Authority, there shall be no piped discharge of surface water from the development prior to the completion of the approved surface water drainage works.

Reason: To ensure that no surface water discharges take place until proper provision has been made for their disposal.

- 36 Unless otherwise agreed in writing by the Local Planning Authority, no building or other obstruction shall be located over or within a twelve metre easement strip centred over the line of Tang Hall Culvert which crosses the site.

Reason: In order to allow sufficient access for maintenance and repair work at all times.

- 37 Notwithstanding the submitted plans, full details of the proposed alignment and extent of the diversion of the Tang Hall culvert shall be submitted to and approved in writing by the Local Planning Authority.

Reason: To ensure that the proposals take into account the works to the culvert associated with the construction of the James Street Link Road.

- 38 Unless otherwise agreed in writing by the Local Planning Authority, no building or other obstruction shall be located within a thirteen metre easement strip centred over the Foss Islands High Level Culvert which crosses the site.

Reason: In order to allow sufficient access for maintenance and repair work at all times.

- 39 Vehicular access shall be provided at all times to the existing chamber and control building compound on the Foss Islands High Level Culvert.

Reason: To enable access to this flood control installation.

- 40 Prior to the commencement of the development hereby approved, a CCTV survey of the Tang Hall Beck overflow culvert shall be undertaken and a remedial strategy shall be developed based upon the findings of this survey. The results of the survey and the remedial strategy shall be submitted to and approved in writing by the Local Planning Authority prior to any development commencing on site

Reason: To ensure that the Tang Hall Beck overflow culvert continues to function satisfactorily.

- 41 Unless otherwise agreed in writing by the Local Planning Authority, no building or other obstruction shall be located within a six metre easement strip centred over the Tang Hall Beck Culvert which crosses the site.

Reason: In order to allow sufficient access for maintenance and repair work at all times.

- 42 For all manholes on the culverts, provision shall be made for machine access and the surrounding areas shall be constructed to adequately support plant and machinery, which may be required to deal with blockages and other maintenance requirements.

Reason: To enable the culverts to be maintained to minimise the risk of blockages occurring.

- 43 Prior to the commencement of the development hereby approved, the routes of all culverts shall be confirmed and located on the topographic survey.

Reason: To ensure that easement strips are correctly located.

- 44 During the construction phase of the proposed development, 24 hour access to the inlet screens on the culverts shall be maintained.

Reason: In the interests of maintenance and flood prevention.

- 45 Prior to works commencing, details shall be submitted and agreed in writing by the Local Planning Authority, of the car park signing necessary as a consequence of the

development. Such signing shall be provided to coincide with the opening of the car parking.

Reason: In the interests of highway safety.

- 46 Prior to works commencing, details of signing of pedestrian and cycle routes both to and from the site shall be submitted and agreed in writing by the Local Planning Authority, such signing shall match the existing signs within the City Centre, and shall be provided before the site comes into use.

Reason: In the interests of highway safety.

- 47 Before development commences the following details shall be submitted to and agreed in writing by the LPA:

- Segregated pedestrian/cycle route through the site
- Security and landscaping adjacent to the cycle route
- Other cycle and pedestrian access to (and potentially through) the site
- Car parking layout, including disabled, parent & child, internal visibility lines.
- Measures to provide safe crossing points within the site, affording priority to pedestrians and cyclists
- Pedestrian guard railing or similar measures
- Bus link through the site, including stop provision.
- Green Corridor

All the above such facilities shall be completed prior to any part of the development being brought into use, or alternatively in accordance with a timetable which has been submitted to and approved in writing by the LPA

Reason: In the interests of safety and good highway management.

- 48 Full details of the proposed CCTV facilities within the site and lighting for the car park and cycle route shall be submitted to and approved in writing by the LPA, prior to any part of the development being brought into use.

Reason: In the interests of safety and visual amenity.

- 49 Prior to works commencing a dilapidation survey shall be jointly undertaken with Officers of the Council, the results of which shall be agreed in writing with the Local Planning Authority.

Reason: In the interests of the safety and good management of the public highway.

- 50 Prior to the development coming into use, all areas used by vehicles shall be surfaced, sealed and positively drained within the site, in accordance with details which have been previously submitted to and approved in writing by the Local Planning Authority.

Reason: To prevent the egress of water and loose material onto the public highway.

- 51 The initial 10m of the access, measured from the nearside highway boundary, shall not exceed a gradient of 1 in 20 (5%). Elsewhere within the site the gradient of pedestrian and vehicular areas shall not exceed 1 in 12 (8.3%).

Reason: To ensure vehicles safely approach and enter the public highway and that the site is accessible to people with disabilities.

- 52 The development shall not come into use until all existing vehicular crossings not shown as being retained on the approved plans have been removed by reinstating the kerbing, footway and verge to match adjacent levels.

Reason: In the interests of good management of the highway and road safety.

- 53 Prior to the development commencing details of the cycle parking areas, including means of enclosure, shall be submitted to and approved in writing by the Local Planning Authority. The building shall not be occupied until the cycle parking areas and means of enclosure have been provided within the site in accordance with such approved details, and these areas shall not be used for any purpose other than the parking of cycles.

Reason: To promote use of cycles thereby reducing congestion on the adjacent roads and in the interests of the amenity of neighbours.

- 54 Prior to the development coming into use the sight lines shown on the approved plans shall be provided free of all obstructions which exceed the height of the adjacent carriageway by more than 1.0m and shall thereafter be so maintained.

Reason: In the interests of road safety.

- 55 Prior to the development commencing details of the measures to be employed to prevent the egress of mud, water and other detritus onto the public highway, and details of the measures to be employed to remove any such substance from the public highway shall be submitted to and approved in writing by the Local Planning Authority. Such measures as shall have been approved shall be employed and adhered to at all times during construction works.

Reason: To prevent the egress of water and loose material creating a hazard on the public highway.

- 56 Prior to the commencement of the use hereby approved, provision shall be made within the site for accommodation of delivery/service vehicles in accordance with details which shall have been previously submitted to and approved in writing by the Local Planning Authority. Thereafter all such areas shall be retained free of all obstructions and used solely for the intended purpose.

Reason: To ensure that delivery/service vehicles can be accommodated within the site and to maintain the free and safe passage of highway users.

- 57 A safety audit (stages 2 and 3) carried out in accordance with the advice set out in the design manual for roads and bridges HD 19/94 and guidance issued by the Council, shall be submitted for the site access(es) and all off-site works, as specified, in the transport assessment.

Reason: In the interests of safety and good highway management.

- 58 All lighting shall be positioned and angled to prevent glare, reflection or distraction to highway users and the illumination shall comply with the recommendations of the Institution of Lighting Engineers Technical Report No. 5.

Reason: In the interests of road safety.

- 59 The development hereby permitted shall not come into use until the following highway works (which definition shall include works associated with any Traffic Regulation Order required as a result of the development, signing, lighting, drainage and other related works) have been carried out in accordance with details which shall have been previously submitted to and approved in writing by the Local Planning Authority, or arrangements entered into which ensure the same.

Reason: In the interests of the safe and free passage of highway users.

The junctions and associated works to Foss Islands Road and the Link Road, as indicated within the transport assessment and submitted drawings by White Green Young.

- 60 The operation and management of all the car park shall be undertaken in accordance with a car park management scheme, which shall be submitted to and approved in writing by the LPA prior to any part of the development being brought into use. The scheme shall specify that parking is limited to a maximum of 2 hours and shall not be revised without the prior approval of the LPA.

Reason: To ensure that the car park management is in accordance with Council policy to control long stay parking.

- 61 The development hereby permitted will not come into use until the James Street Link Road (phase 1) from James Street to Layerthorpe is completed and brought into operation.

Reason: In the interests of good management of the highway and road safety.

7.0 INFORMATIVES:

Notes to Applicant

1. 7. REASON FOR APPROVAL

In the opinion of the Local Planning Authority the proposal, subject to the conditions listed above, would not cause undue harm to interests of acknowledged importance, with particular reference to retail capacity, the environmental impacts and the impact on the highway network. As such the proposal complies with Policies S3 and S4 of the North Yorkshire County Structure Plan (Alteration No.3 Adopted 1995) and Policies SP7, SP8, S1, S2, HE4, NE7, and NE8 of the City of York Local Plan Deposit Draft.

2. Notes to Applicant

1. In preparing the landscaping scheme required by Condition 13, the developer is encouraged to liaise with YNET. The Local Planning Authority would coordinate these discussions.

2. The developer's attention should be drawn to the Council's consideration that the indicative plan is unacceptable, with particular reference to the objective of providing a green corridor through the site.

3. You are advised that prior to starting on site consent will be required from the Highway Authority for the works being proposed under Section 278 of the Highways Act 1980 (Highway Works).

4. The developer's attention should also be drawn to the various requirements for the control of noise on construction sites laid down in the Control of Pollution Act 1974. In order to ensure that residents are not adversely affected by air pollution and noise, the following guidance should be attached to any planning approval:

A. The work shall be carried out in such a manner so as to comply with the general recommendations of British Standards BS 5228: Part 1: 1997, a code of practice for "Noise and Vibration Control on Construction and Open Sites" and in particular Section 10 of Part 1 of the code entitled "Control of noise and vibration".

B. All plant and machinery to be operated, sited and maintained in order to minimise disturbance. All items of machinery powered by internal combustion engines must be properly silenced and/or fitted with effective and well-maintained mufflers in accordance with manufacturers instructions.

C. The best practicable means, as defined by Section 72 of the Control of Pollution Act 1974, shall be employed at all times, in order to minimise noise emissions.

D. All reasonable measures shall be employed in order to control and minimise dust emissions, including sheeting of vehicles and use of water for dust suppression.

E. Any asbestos containing materials shall be removed by licensed contractors to a licensed disposal site.

F. There shall be no bonfires on the site.

5. The remedial strategy shall have due regard for UK adopted policy on risk assessment and shall be developed in full consultation with the appropriate regulator(s).

6. Additional mezzanines floors within the non food element of the scheme will be encouraged should this result in a significant reduction in the footprint of the building.

7. You are advised that this proposal may have an affect on Statutory Undertakers equipment. You must contact all the utilities to ascertain the location of the equipment and any requirements they might have prior to works commencing.

Contact details:

Author: Matthew Parkinson Development Control Officer

Tel No: 01904 552405

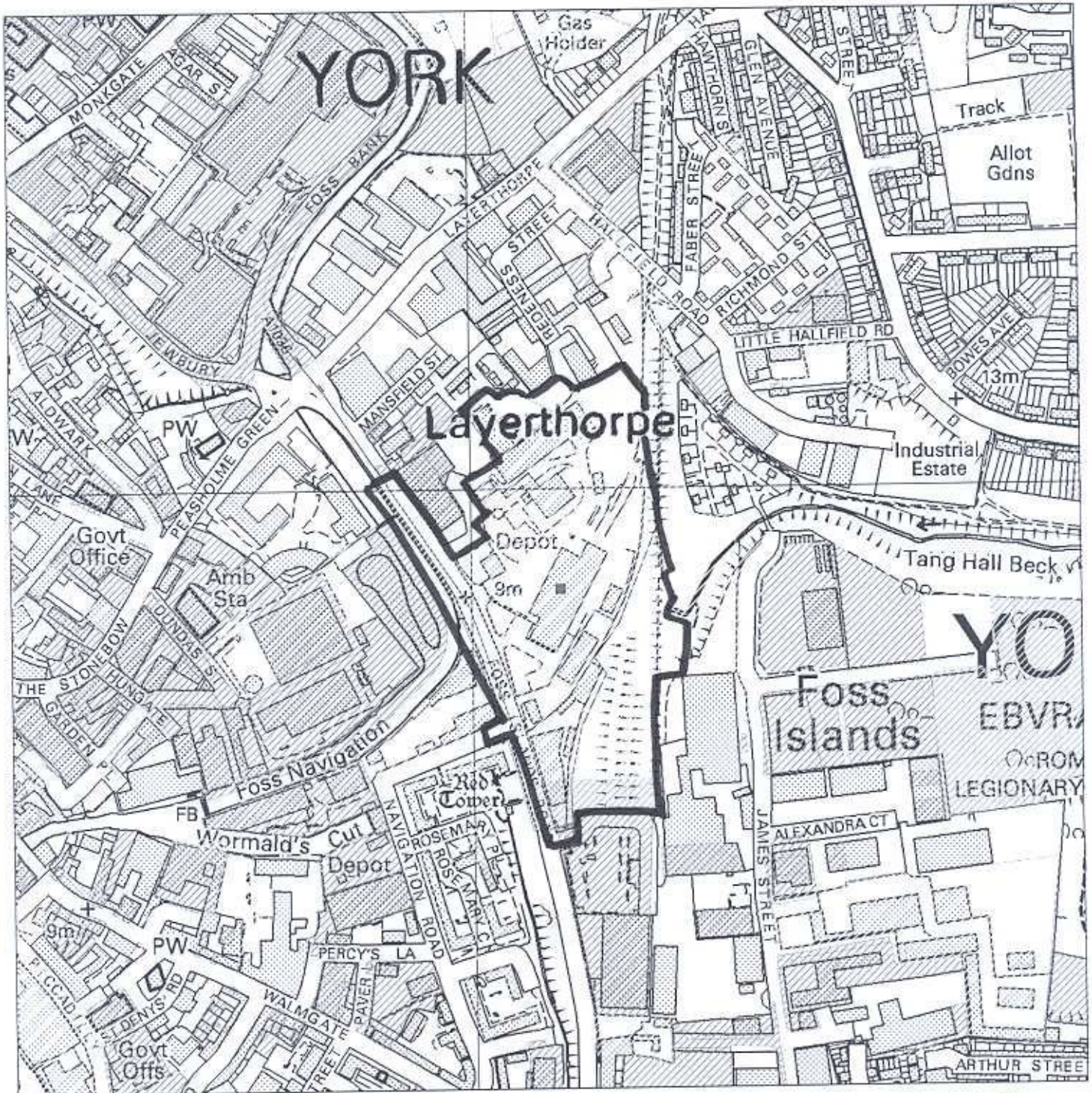
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Foss Islands Road

06/00338/GRG3 and 06/00333/REMM



GIS by ESRI (UK)



Scale : 1:5000

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Organisation	City of York Council
Department	Development Control
Comments	
Date	18 July 2006
SLA Number	

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1.5 The outline planning permission was issued on 9th December 2004 following referral of the application to the Government Office for Yorkshire and Humber, the completion of a Section 106 Agreement. The outline application, which included details of the siting and means of access for consideration at that stage established the principle of the development and set limitations on the amount of food and non food retail floorspace, as follows:-

- Non-food retail units with a footprint of 6,970 sq m (75,000 sq ft) and gross floorspace of 10,220 sq m (110,00 sq ft)
- A foodstore of 6,718 sq m (82,000 sq ft) for occupation by Morrison Supermarkets, to include a petrol filling station.
- A restaurant of 372 sq m (4,000 sq ft)
- Relocation of Kwik Fit 643 sq m (6,830 sq ft).

1.6 That outline permission was then amended by planning permission 04/04420/GRG4 dated 24th February 2004. This was also a Section 73 application and was sought in order to allow the redevelopment of the site in phases and introduced a phasing condition to allow for this. A number of the other conditions were also amended. A further outline permission was then issued on 2nd September 2005 which further varied condition 4 (non-food sales floorspace be limited) of 04/04420/GRG4

1.7 A reserved matters application was submitted in December 2004 illustrating the same layout and footprint as that indicated at the time of the outline application, but also including details of the cycle, bus and pedestrian route onto and through the site from Foss Islands Road to the James Street Link Road. This is the only reserved matters approval granted so far.

2.0 POLICY CONTEXT

2.1 Development Plan Allocation:

Listed Buildings Grade 2; Chimney At Works Depot Foss Islands Road 0024

Floodzone 3 Flood Zone 3

Floodzone 2 Flood Zone 2 CONF

DC Area Teams Multiple (Spatial)

City Boundary York City Boundary 0001

Conservation Area Central Historic Core 0038

Areas of Archaeological Interest City Centre Area 0006

Listed Building - Grade II

2.2 Policies:

CYGP1
Design

CYGP3
Planning against crime

CYGP4
Environmental sustainability

CYGP11
Accessibility

CYHE2
Development in historic locations

CYT4
Cycle parking standards

CYT13
Car park standards in York CC/District C

CYHE7
Security shutters in historic locations

CYNE8
Green corridors

3.0 CONSULTATIONS

3.1 INTERNAL.

3.2 Highway Network Management.
No objections raised.

Whilst the application increases the ground floor area of non-food retail, the figure is still below that which was considered as part of the Foss Basin masterplan and as such is considered acceptable.

3.3 Environment and Conservation .

The Morrisons superstore building has some elevational changes which bring the two storey element aspect of the southern elevation closer to the listed chimney and a slightly more solid frontage wall where previously there was an overhanging canopy. However, given the strong vertical emphasis of the chimney and the fact that the seating and landscaping around the chimney remains the same, these changes are not considered to affect the setting of the listed chimney.

Changes to the non-food retail terrace of buildings are acceptable and have no impact visually from outside the site.

3.4 Landscape Officer.
No objections.

3.5 Structures and Drainage.

Insufficient information has been provided at this stage by the Developer to determine the potential impact the proposals may have on the existing drainage systems. Details are therefore awaited before construction commences to satisfy the drainage conditions placed on the original permissions.

3.6 Environmental Protection Unit
No comments in respect of this application.

EXTERNAL.

3.7 Heworth Planning Panel.
No objections.

3.8 Environment Agency.
The Flood Risk Assessment dated Feb 2004 concludes that Finished Floor Levels should be set at least to the minimum agreed level of 10.30 metres above ordnance datum. If the Morrisons building is to have floor levels of 10.30AOD then this remains acceptable. Lower would not be acceptable.

All comments made in previous correspondence as to the design and installation of the culvert are still applicable.

3.9 Yorkshire Water.
No comments to make on this application. Original comments and conditions apply.

3.10 English Heritage.
No comments to make.

3.11 Foss Internal Drainage Board.
Has no objections to the proposals, however the planning and drainage authorities are reminded that no works undertaken by the developer of the site should be allowed to have a detrimental effect on water levels on the River Foss or the Osbaldwick/Tang Hall Beck.

3.12 York Cycle Campaign.
Object. Plans do not make the most of the opportunity to link up the Foss Islands cycle route with a possible link into the city centre, possibly through the new Hungate development. The development could be excellently served by high quality cycle routes which provide a much needed traffic free access into the city centre from the residential areas further out such as Heworth, Tang Hall and Osbaldwick. The plans presented do not make the most of this opportunity and the cycle facilities provided are badly designed and unattractive.

- Development has been designed as a car based development.
- impact on traffic generation
- wider effects on local shops in the York area.
- non-cyclists often won't cycle because of volume of traffic and this development can only increase this.
- cycle and pedestrian facilities give the impression of not being very well thought through - they are indirect and discontinuous.
- unclear where / how cyclists will get on and off the route.
- unclear how they will get onto the future link road.
- unclear how cyclists will access the proposed cycleway along the Foss or from the new Hungate development. This appears to have been ignored but could be a huge benefit to the city.
- unclear how cyclists access the cycle parking for the supermarket via the cycle route.
- proposed green corridor is not sufficiently wide to provide a continuous, fully viable 'green corridor' or a pleasant environment for cyclists or pedestrians. Understand that this was originally agreed upon by the planning committee.

3.13 York Natural Environment Trust.
States that the way these applications have been presented, that is simply as minor amendments intended to consolidate conditions and plans for the Morrisons scheme to be an attempt to obtain by stealth, planning consents for what amounts to a new scheme. It should be subject to a new outline application which only if approved should then require a

new reserved matters application. Applications should be subject to a full public consultation in a form which makes their true intent clear and should ultimately be referred to the Sec of State for approval.

- plans lack clarity and fail to provide a clear key. Scale is misleading.
- drainage plans are unsustainable.
- width of the tree belt alongside Foss Islands Road specified in the original application as 10 metres now averages only 7 metres and falls as low as 5 in places. This is insufficient to provide the depth of ameliorating and screening originally specified and will be inadequate for its purpose.
- the green corridor is inadequate. In both the previous and this application it appears as only a transport corridor with some disjointed elements of embellished landscaping provided.
- as a route for people, the footway / cycleway is disjointed and made hazardous by being mixed up with motor vehicle roads. Will not be a pleasant experience for people using it as a link between the river foss corridor and St. Nicholas fields.
- During consideration of the 'Morrisons' scheme, it became clear that the failure of the green corridor concept had resulted from failure to resolve at the outline stage, conflicts between space needed for landscape, and habitat provision, space needed for a safe and pleasant pedestrian/cycle route, space required for a bus route and space aspired to by developers to maximise car parking. It is essential in the new 'Morrisons/Homebase' scheme that these conflicts are resolved and provision made for a continuous, fully viable green and wildlife corridor, accommodating a safe and pleasant pedestrian/cycle route, segregated from motor traffic, before the outline application is determined.

4.0 APPRAISAL

4.1 Section 73 of the 1990 Town and Country Planning Act provides for applications for planning permission without complying with conditions previously imposed on a planning permission. The Local Planning Authority can grant such permissions unconditionally or subject to different conditions, or they can refuse the application if they decide the original conditions should continue in the same form. The proposal here is to vary some of the original conditions and if approved, a completely new decision notice would be issued.

4.2 The key issues are whether the proposed alterations in design and layout are acceptable and whether the proposed variation of the wording allows the Council to retain the necessary control over the development in terms of details and timescale. The precision and enforceability of any reworded conditions is also a key consideration.

4.3 Only 5 conditions were attached to the reserved matters permission 04/04381/REM and only condition 1 of these is sought for amendment here. This referred to the schedule of approved plans and cover all detailed aspects of the scheme. However, these approved plans include the detailed site layout and elevations plans of all the buildings and therefore the details controlled by this condition are important and far reaching. The changes are detailed below.

Supermarket.

4.4 The position and footprint of the supermarket and the retail floorspace area remains identical to that approved (7618sqm). The changes proposed are cosmetic and come about because the internal first floor accommodation within the building has been moved further west (towards Foss Islands Road) and this results in alterations to the appearance of the elevation facing the car park by moving the two storey element much closer to the secondary entrance on the south western corner of the building. This secondary entrance is important as it is adjacent to the listed chimney, shown on the approved scheme to be circled by seating with a paved and landscaped area to the rear. The changes to this secondary

entrance are relatively minor but whilst they don't bring the nearest part of the supermarket any closer to the chimney, a solid wall replaces space previously underneath the overhanging canopy of the building (the canopy has now been deleted). Whilst this is regrettable and one would have hoped, avoidable, the actual additional impact on the listed chimney is minor. The vertical emphasis of the chimney is such that it remains the dominant structure and therefore the proposed alterations to the Morrisons facade at this point does not materially alter this relationship. The seating, paving and landscaped area around the chimney are unaltered and so officers have concluded that there is no greater impact on the setting of the listed chimney as that approved.

4.5 The main entrance is at the other end of the same elevation facing towards the transport corridor. This main entrance is unchanged.

4.6 The building follows the same design principles throughout and these are the same as approved. They comprise facing brickwork with contrasting horizontal banding and coloured panels at high level. The roofing over the two storey offices and the mansard roofing are to be in natural slate with an aluminium standing seam roof to the main entrance and 'towers'. The alterations are very minor and it remains acceptable in terms of its external appearance and design.

Non-food retail area.

4.7 This is the area of most change. The issues surrounding the slight alterations to the unit sizes and consequent retailing issues are considered in the previous report and are not relevant here. The issue here is the alteration in design and appearance brought about as a result of these changes.

4.8 The incorporation of Homebase into the scheme requires a different footprint for this non-food retail 'terrace'. The approved layout shows 6 retail units of differing, but fairly equal, sizes. The trading requirements of Homebase means that they will occupy a much larger unit area and this impacts not only on the Homebase unit itself but also on the layout of the other units as well. The size of Homebase means that only 4 separate units will now occupy this terrace and consequently elevation changes are required to reflect this. Additionally, there is also the need to accommodate the new garden centre attached to Homebase.

4.9 The footprint of this terrace alters because the unit occupied by Homebase will be slightly deeper and longer than that approved. However the main changes to the footprint are to the rear of the building adjacent to the sites eastern boundary with adjacent warehousing. The turning area for service vehicles has moved further south to behind the relocated Kwik Fit building but officers do not consider this to be unacceptable and not materially harm the overall appearance of the site.

4.10 The incorporation of this larger unit also has consequent changes to the front of this terrace as store entries and the relationship of these to the first line of car parking in front of the stores need to be modified. It must be stressed however that the length of the building remains the same and from the main public views within the site and from Foss Islands Road, the building will appear almost identical in appearance to that approved. For the information of members the general appearance of this row of buildings is as follows:

4.11 The front elevations of the non food retail units and the motorist centre would be in a single plane, varying in distance from Foss Islands Road boundary between 36 metres to the south and 88 metres at the northern end. They would measure 8.2 metres to eaves. In a similar vein to the Monks Cross retails development, each unit would have taller tower feature entrances with signage above the entrance doors. The entrance features would project to 10.2 metres above ground level. The elevations would be finished in silver

coloured cladding panels. The plinth would be in faced blockwork. The roof would be of composite panelling, coloured goosewing grey. A glazed canopy projecting some 2.5 metres from the face of the retail terrace building would run along the front elevation.

4.12 The Motorist repair centre.

Very minor change replacing the five doors on the southern elevation with four. No impact.

4.13 The Petrol Filling Station.

This remains in the same place on the site and access into it is the same. The extent of the canopy has been reduced and the general layout reconfigured slightly. An LPG storage area has been replaced with a jet wash. These changes are limited and acceptable. Indeed it offers improvements inasmuch that it widens the landscaped strip close to the site entrance off Foss Islands Road and additional tree planting is shown in here which will assist further in screening views of the petrol station areas from Foss Islands Road.

4.14 The restaurant.

The freestanding restaurant is 4000sqm in size and is located just behind the 10 metre wide landscaped belt adjacent to Foss Islands Road. The approved scheme shows a contemporary structure, 4.1 metres high to eaves level, with glazing and brickwork to the elevations, and a flat roof in composite panels coloured goosewing grey. Given the relatively low roof height of the structure it was considered acceptable as it would have a limited visual impact along Foss Islands Road. The proposed amendment introduces a different design which is higher and offers accommodation over 2 floors. It also significantly alters the design and shows an entrance feature similar to those on the non-food retail terrace to the east. This change was considered unacceptable by officers as it makes the restaurant much more visually noticeable from Foss Islands Road and results in a design completely at odds with the simple, lightweight design previously approved. As a result the applicant has agreed to withdraw this change at this stage and will instead resort back to the approved design. For the purposes of this application therefore, there are no alterations to the restaurant.

4.15 Central Green / Wildlife Corridor.

The extent, layout and character of this remains unaltered.

4.16 Landscape Belt to Foss Islands Road.

The extent of this landscaped belt running along the extent of the Foss Islands Road site frontage remains unaltered.

4.17 Highway layout.

There are a number of very minor alterations to the car park layout but the number of spaces remains as approved. Cycle and pedestrian routes are unaltered and there is no additional impact on highway, cyclist or pedestrian safety as a result of these amendments. Details conditioned and these remain unaltered and will require discharging before development commences.

4.18 Drainage.

No change to the approved plans. Details conditioned and these remain unaltered and will require discharging before development commences.

5.0 CONCLUSION

5.1 The changes are relatively minor and mainly consolidate the existing approval for the details of landscaping, siting and external appearance of the development. These are the only factors for determination here. The main changes are to the non-food retail element of

the proposal but these do not materially affect the layout of the site or the appearance of the building. The important landscaping details remain largely unaltered.

5.2 Given that the changes are relatively minor, it is considered that the details can be altered and agreed by substituting the approved plans for those submitted here. This requires an alteration to the schedule of plans condition 1. Conditions 2-5 remain unaltered and will be re-appended.

5.3 Subject to:-

a) The Secretary of State allowing the Authority to determine the variation of the outline application 06/00338/GRG3. (previous item on agenda)

b) Variation of the S. 106 Agreement to refer to this variation application

a) The conditions as set out below and supplementing those on reserved matters approval 04/04381/REM,

it is recommended that the application be approved. The changes are highlighted in bold type for ease of reference.

6.0 RECOMMENDATION: Approve subject to Section 106 Agreement

- 1 **The development shall not be carried out otherwise than in complete accordance with the plans as originally submitted and later amended by the revised drawings listed below and received on the date indicated, or as may otherwise be agreed in writing with the Local Planning Authority.**

Dwg No. ELBFIR/D006 rev.B (Halcrow Yolles) received on 9th February 2006

Dwg No. PL207.M.03 N (Planit) received on 9th February 2006

Dwg No: 3527-24-GA03 (Watson Batty) received on 28th March 2006

Dwg No: 3527-24-03 V (Watson Batty) received on 28th March 2006

Dwg No: 3527-24-PFS 05 & 08 (Watson Batty) received on 28th March 2006

Dwg No: 3527-24-06A (Watson Batty) received on 9th February 2006

Dwg No: 3527-24-07A (Watson Batty) received on 9th February 2006

Dwg No: 3527-24-08A (Watson Batty) received on 9th February 2006

Dwg No. 201 N (Harris) received on 9th February 2006

Dwg No. 252 F(Harris) received on 9th February 2006 (excluding details of motorist repair unit - see 212 B)

Dwg No. 209 D (Harris) received on 9th February 2006

Dwg No. 251 F (Harris) received on 9th February 2006

Dwg No. 242 (Harris) received on 9th February 2006

Dwg No. 250 G (Harris) received on 9th February 2006

Dwg No. 241B (Harris) received on 9th February 2006

Dwg No. 212 B (Harris) received on 13th April 2006

Dwg No. 207B (Harris) received on 15th February 2005 and approved 17th March 2005 (restaurant unit as originally approved)

- 2 Large scale details of the items listed below shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of the development and the works shall be carried out in accordance with the approved details.

- Entrances to retail units and motorist centre
- Seating around base of listed chimney
- Details of pedestrian/cycle routes at crossing points with access roads

Reason: So that the Local Planning Authority may be satisfied with these details.

- 3 Notwithstanding any proposed materials specified on the approved drawings or in the application form submitted with the application, samples of the external materials to be used shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of the development. The development shall be carried out using the approved materials.

Reason: So as to achieve a visually cohesive appearance.

- 4 A sample panel of the brickwork to be used on this building shall be erected on the site and shall illustrate the colour, texture and bonding of brickwork and the mortar treatment to be used, and shall be approved in writing by the Local Planning Authority prior to the commencement of building works. This panel shall be retained until a minimum of 2 square metres of wall of the approved development has been completed in accordance with the approved sample.

Reason: So that the Local Planning Authority may be satisfied with the finished appearance of these details prior to the commencement of building works in view of their sensitive location.

- 5 Notwithstanding the details illustrated on the submitted plans, the high level culvert chamber compound adjacent to Foss Islands Road shall be bounded by brickwork walls, in accordance with details which shall have previously been agreed in writing by the Local Planning Authority.

Reason : In the interests of the visual amenities of the locality and security of the compound.

7.0 INFORMATIVES:

Notes to Applicant

1. REASON FOR APPROVAL

In the opinion of the Local Planning Authority the proposal, subject to the conditions listed above, would not cause undue harm to interests of acknowledged importance, with particular reference to highway safety, visual amenity, setting of the listed building, drainage and flooding. As such the proposal complies with Policies of Regional Planning Guidance Note 12, Revised Spatial Strategy for Yorkshire and Humber (2004), Policies T10 and E4 of the North Yorkshire County Structure Plan (Alteration No.3 Adopted 1995) and Policies GP1, GP3, GP4, GP11, HE2, T4, T13, NE7 and NE8 of the City of York Local Plan Deposit Draft.

Contact details:

Author: Matthew Parkinson Development Control Officer
Tel No: 01904 552405

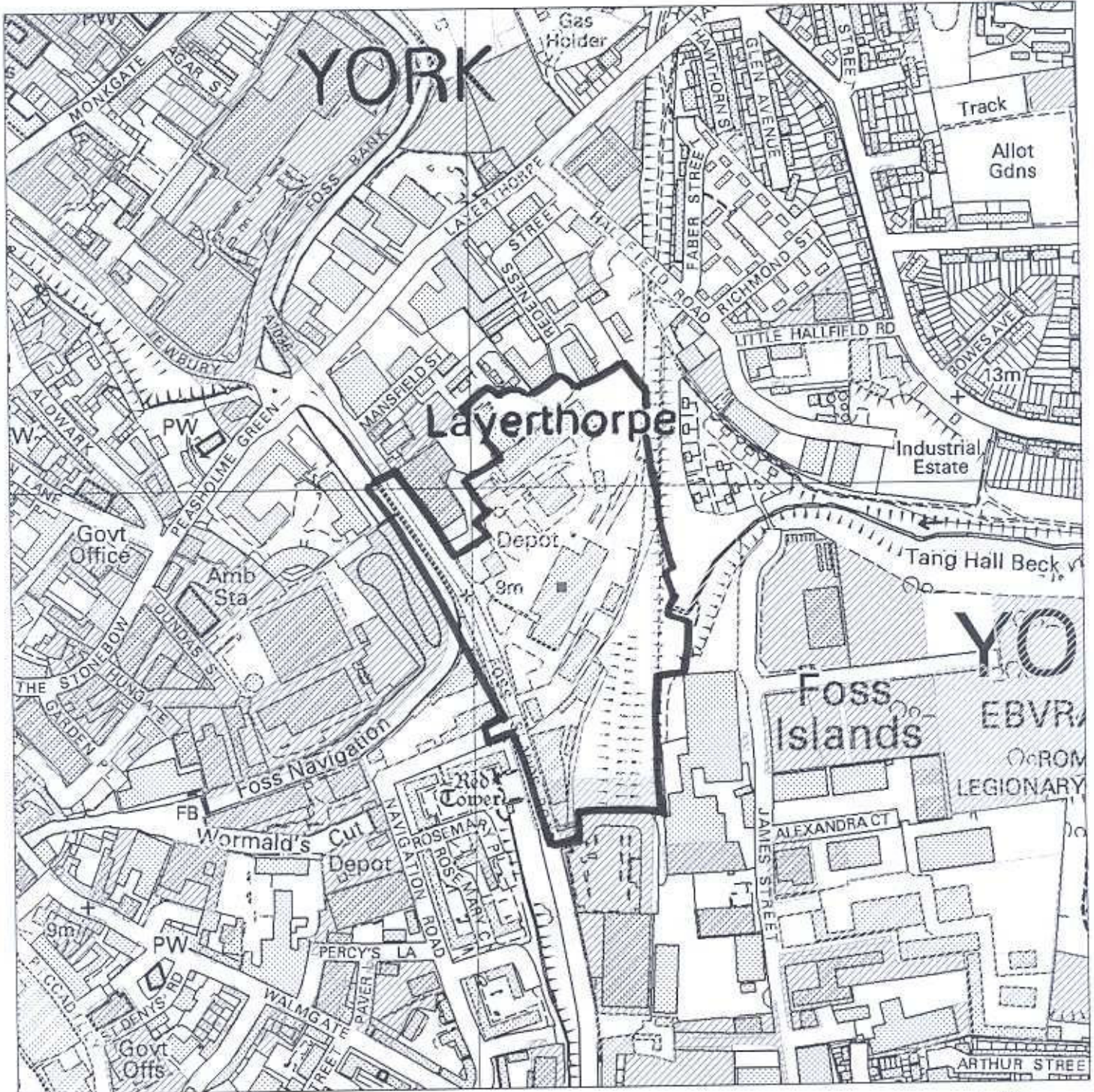
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Foss Islands Road

06/00338/GRG3 and 06/00333/REMM



GIS by ESRI (UK)



Scale : 1:5000

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Organisation	City of York Council
Department	Development Control
Comments	
Date	18 July 2006
SLA Number	

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Planning Committee

27 July 2006

Report of the Director of City Strategy

THE RACECOURSE AND TERRY'S FACTORY CONSERVATION AREA CHARACTER APPRAISAL: CONSULTATION DRAFT

Summary

1. This report asks that a draft consultation character appraisal for the Racecourse and Terry's Factory Conservation Area be approved for consultation. This document is contained within Annex C.

Background

2. A development brief for the Terry's Factory site was approved for use as non-statutory supplementary planning guidance by Planning Committee on 27 June 2006. Section 3.19 of the Brief specifies that a number of studies should be compiled to aid the master-planning of the site. One of these required documents is a full conservation area character appraisal, produced in line with English Heritage Guidance (Guidance on Conservation Area Appraisals, English Heritage 2006).
3. The Terry's Factory site partly lies within the Racecourse and Terry's Factory Conservation Area. This conservation area was designated in 1975 in recognition of two important landmark sites to the south of the city. The boundary is closely wrapped around the more historic built sections of each site but not the surrounding landscape, such as the racetrack. A map of the conservation area is contained within Annex A.
4. A conservation area appraisal describes in detail, the special architectural and historic character of a conservation area. By doing so it explains why an area is worthy of designation and provides clear information on which to formulate and decide development proposals. Such a document is advocated in PPG15 Planning and the Historic Environment 1995, Section 4.4:

'The more clearly the special architectural or historic interest that justifies designation is defined and recorded, the sounder will be the basis of local plan policies and development control decisions, as well as for the formulation of proposals for the preservation and enhancement of an area.'

5. Best Value Performance Indicator BVPI 219b measures the percentage of conservation areas in the local authority area with an up-to-date character appraisal. There are 34 conservation areas within the District, with only one approved appraisal, the Castle Piccadilly Conservation Area Appraisal (Approved by Planning Committee on 23 March 2006). The 2006/2007 target is for two character appraisals to be produced and approved.
6. The appraisal has been funded by the developers of the former Terry's site, GHT Developments LLP and produced by Woodhall Planning & Conservation of Leeds. The format and content of the draft consultation document (Annex C) has been approved by Council officers in the Design, Conservation & Sustainable Section. If approved the appraisal will become an official Council Document.
7. The appraisal discusses the whole conservation area, not just the part affected by the redevelopment of the former Terry's factory site. Its main points are:
 - The buildings of the Racecourse and former Terry's factory site are major landmarks within the south side of the city. Both sites have strong historical relevance to the city and its residents, as well as national and international significance.
 - The open landscape setting of the area, formed by the Knavesmire and Nun Ings is of great importance in forming character.
 - Some buildings, such as the Factory Clock tower or the larger grandstands dominate the skyline of this part of the city.
 - The conservation area can be divided into three sub character areas; the Racecourse, the former Terry's factory site and Park Pavilion Field (See Annex A). The first two areas contain a significant number of listed buildings but also more recent development, the character of which is mixed. The factory site is architecturally cohesive whilst the racecourse comprises a greater variety of styles and designs, albeit unified by a common purpose.
 - The character of the racecourse changes markedly according to whether there is a race meet or not.
8. A list of proposed consultees can be found in Annex B. Except for ward members, ward committees and heritage or planning interest groups, summary leaflets only will be sent. Copies of these leaflets will be handed out at the Planning Committee meeting. 2300 households in South Bank will receive the leaflets and comments invited on the contents of the leaflet with a further invitation to view and comment upon the whole document. Electronic as well as paper responses will be accepted. In addition to the Council's website hard copies of the appraisal will be deposited in local libraries, council offices and community or church centres (subject to agreement). A press release will be made.

9. It is envisaged that many people, especially in South Bank, will have personal memories of working at the Terry's factory. In order to recognise this important social element to the site's history, it is hoped that information drawn from the Terry's archive and a recent project by York Oral History Society on working at the factory can be included within the appraisal as an appendix. To achieve this Officers will liaise, during the consultation period, with the York Castle Museum and the York Oral History Society.
10. If Members approve the recommendation of the report, consultation will start on the 7 August 2006 and end on 8 September 2006. This long time period takes into account the summer holiday season. A revised appraisal will then be presented to Planning Committee on 26 October 2006.

Consultation

11. No one has been consulted on this report or document. Rather, this report seeks approval for a consultation process.

Options

12. Option 1

Approve the Racecourse and Terry's Factory Conservation Area Character Appraisal (Consultation Draft), as proposed in this report, for use as a consultation document.

Option 2

Approve the Racecourse and Terry's Factory Conservation Area Character Appraisal (Consultation Draft), but with modifications to its content.

Option 3

Do not approve the Racecourse and Terry's Factory Conservation Area Character Appraisal (Consultation Draft) and halt its production altogether.

Analysis

13. Option 1

It is felt that the document contained with Annex C is a thorough analysis of the character of the Conservation Area, in line with current guidance from English Heritage. As a consultation document it is clearly written and capable of amendments where required. The final adoption of this document will help the formulation and determination of development proposals within the conservation area and especially the Terry's Factory site. The document will help the Council meet the yearly target set by BVPI 219b (See paragraph 5 above).

Option 2

As for option 1 analysis with the caveat that significant delays in the re-drafting of the document may compromise the projected consultation timetable. This in turn may compromise the Council's and others ability to consider the redevelopment of the former Terry's factory site.

Option 3

Not approving the document will be contrary to Section 3.19 of the Terry's Factory Site Planning Brief (Approved by Planning Committee on 27 June 2006. It would compromise the ability of the Council and others to fully consider the redevelopment of the Terry's factory site and development in other parts of the conservation area and its surroundings. Considering current internal resources, it is unlikely that the target set by BVPI 129b will be achieved this year.

Corporate Priorities

14. The Racecourse and Terry's Factory Conservation Area Character Appraisal will help improve the actual and perceived condition and appearance of the City, by providing a more informed base for decision making. With particular regard to the redevelopment of the Terry's site, the document will contribute to the growth of Science City York.

Implications

15. **Financial**

Production of consultation documents and delivery costs will be met by existing budgets.

16. **Human Resources (HR)**

No implications

17. **Equalities**

Different formats of the consultation leaflet and appraisal will be made available on request.

18. **Legal**

No implications

19. **Crime and Disorder**

No implications

20. **Information Technology (IT)**

No implications

21. **Property**

No implications

Risk Management

22. There are no known risks.

Recommendations

23. Members are asked to approve the Racecourse and Terry's Factory Conservation Area Character Appraisal (Consultation Draft), as proposed in Annex C of this report, for use as a consultation document.

Reasons:

- The document is a thorough analysis of the character of the Conservation Area, in line with current guidance from English Heritage. As a consultation document it is clearly written and capable of amendments where required.
- The final adoption of this document will help the formulation and determination of development proposals within the conservation area and especially the Terry's Factory site.
- The document will help the Council meet the yearly targets set by BVPI 219b (See paragraph 5 above).

Contact Details

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Design, Conservation &
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Directorate of City Strategy
01904 551329

Chief Officer Responsible for the report:

Mike Slater
Assistant Director of Planning & Sustainable
Development, City Strategy

Report Approved **Date** 13-07-06

Chief Officer's name
Title

Report Approved **Date**

Specialist Implications Officer(s)

Wards Affected: Micklegate, Bishopthorpe, Dringhouses and Woodthorpe

All

For further information please contact the author of the report

Background Papers:

Terry's Factory Site Development Brief, City of York Council
Guidance on Conservation Area Appraisals, English Heritage 2006
PPG15 1995 Planning and the Historic Environment

Annexes

Annex A: Map of The Racecourse and Terry's Factory Conservation Area

Annex B: List of proposed consultees

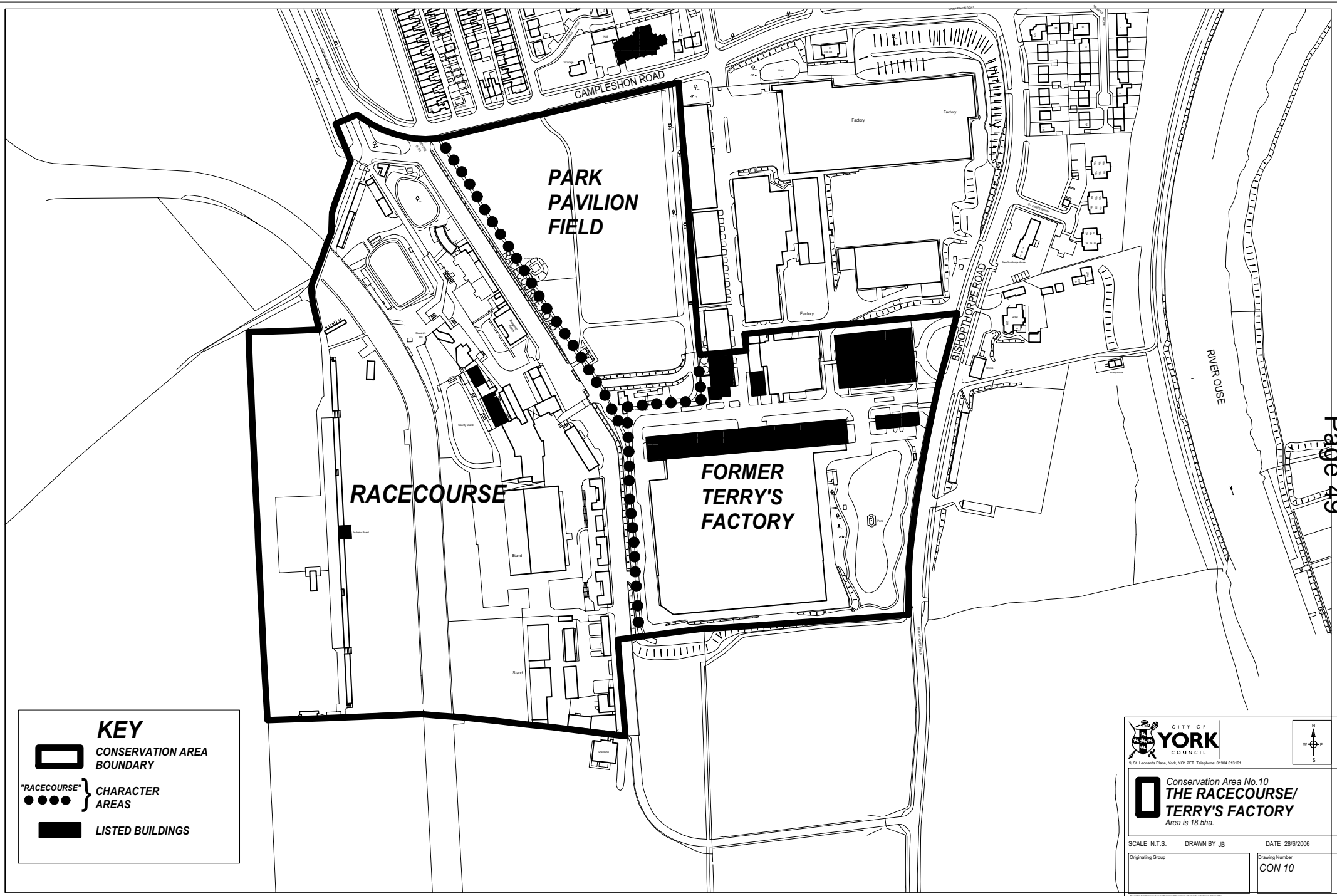
Annex C: The Racecourse and Terry's Factory Conservation Area Conservation Area Appraisal Character Appraisal, Consultation Draft

NOTE:




Annex C to this report has colour maps, photos and plans. Colour copies of this are available as follows:

- Online at www.york.gov.uk (see meetings).
- For inspection upon request at the Guildhall, St Helen's Square, York, YO1 9QN. Please ask for Rowan Hindley.
- A copy is available in the Members Library for Members.

Colour copies will be available at the meeting.



KEY

-  CONSERVATION AREA BOUNDARY
-  "RACECOURSE" CHARACTER AREAS
-  LISTED BUILDINGS

 <p>CITY OF YORK COUNCIL <small>1, St. Leonards Place, York, YO1 2ET Telephone: 01904 613161</small></p>		
<p>Conservation Area No.10 THE RACECOURSE/ TERRY'S FACTORY <small>Area is 18.5ha.</small></p>		
SCALE N.T.S.	DRAWN BY JB	DATE 28/6/2006
Originating Group	Drawing Number	CON 10

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ANNEX B: CONSULTEE LIST - THE RACECOURSE AND TERRY'S FACTORY CONSERVATION AREA CHARACTER APPRAISAL

ORGANISATION	NAME	POSITION
Cllr Evans		Micklegate Ward Member
Cllr Fraser		Micklegate Ward Member
Cllr Holvey		Dringhouses & Woodthorpe Ward Member
Cllr Livesley		Bishopthorpe Ward Member
Cllr Merrett		Micklegate Ward Member
Cllr Reid		Dringhouses & Woodthorpe Ward Member
Cllr Sunderland		Dringhouses & Woodthorpe Ward Member
Cllr Hill		Fishergate Ward Member
Cllr D'Agorne		Fishergate Ward Member
Cllr Aspden		Fulford Ward Member
Bishopthorpe Local History Group	Linda Haywood	
Clementhorpe Community Association	Derek Shaw	Chair
Dringhouses Local History Group	Mrs Elizabeth Smith	Chair
Grantside Limited/GHT Developments LLP		
Knavesmire Primary School	Mr B Schofield	Chair of Governors
Local Business'		
Residents	Deliver to 2100 + post to those on 'keep informed' list	
Scarcroft Residents Association	Mrs C Oakenfull	Chair
South Bank Social Club		
South Bank Surgery		
St Chads Church	Revd Simon Stanley	
St Clement's Working Men's Club		
The College of Law		

York Open Planning Forum	Alison Sinclair	Chair
York Racecourse Committee	Mr W Derby	Chief Executive and Clerk of the Course
Middlethorpe Hall & Spa	Richard Broyd	
Bishopthorpe Parish Council	Mrs C Godfrey	Clerk
Conservation Area Advisory Panel	c/o Verlie Riley	
Council for British Archaeology		
CPRE (York and Selby District)	Dr G Woolley	Chair
Dringhouses and Woodthorpe Planning Panel	Mr Jennings	Clerk
English Heritage	Mr D Fraser	Regional Director
Micklegate Planning Panel	Mr T Martin	Clerk
York Archaeological Forum	Mr Steve Roskams	
York Civic Trust	Mr P Brown	Director
York Philosophical Society		
Yorkshire Architectural and York Archaeological Society	Mr Ivan Martin	
CYC City Development	Martin Grainger	Principle Forward Planning Officer
CYC City Development	Ewan Taylor	City Development Officer
CYC DCSD	Esther Prestley	Landscape Architect
CYC DCSD	John Oxley	City Archaeologist
CYC Development Control	Gareth Arnhold	Area team Leader

**The consultation period falls between the Ward Committee Cycle.
The following ward committees will be sent consultation
information for reference only: Micklegate, Bishopthorpe & Woodthorpe, Dringhouses
and Woodthorpe, Fishergate, Fulford**

THE RACECOURSE AND TERRY'S FACTORY CONSERVATION AREA CHARACTER APPRAISAL



The racecourse enclosure and the former Terry's Factory are two very different groups of buildings on the open space of the Knavesmire to the south of York city centre, representing important aspects of the city's history. Annual race meetings began on the Knavesmire in 1731, continuing a tradition of horse racing within York that can be traced back to the sixteenth century. Chocolate making in York dates from 1767, although the impressive buildings on Bishopthorpe Road were not erected until the 1920s. The buildings of both the racecourse and the factory are major landmarks on the south side of the city.

**Prepared by Woodhall Planning and Conservation
June 2006**

Consultation draft



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APPENDIX - Listed Building Descriptions

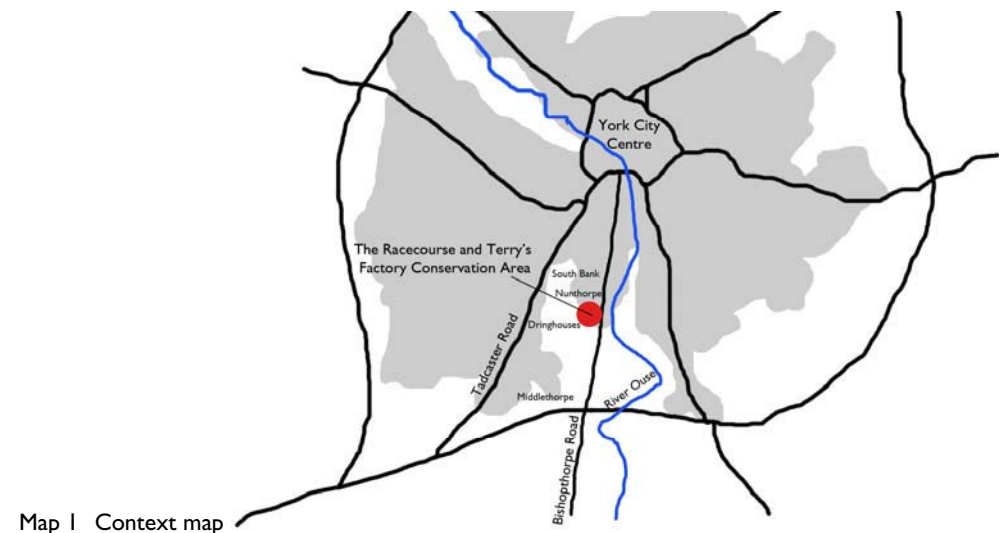
This Conservation Area Character Appraisal has been produced prior to the redevelopment of the former Terry's factory site, Bishopthorpe Road, York. The document was funded by the developers of the site, GHT Developments LLP, and written by Woodhall Planning and Conservation, Leeds. The format and initial content was agreed with City of York Council Officers within the Design, Conservation and Sustainability Section and the document approved for consultation by City of York Council Planning Committee on 27th July 2006.

I.00 INTRODUCTION

- I.01 Section 69 of the Planning (Listed Buildings and Conservation Areas) Act 1990 requires local planning authorities to designate as conservation areas any “areas of special architectural or historic interest the character or appearance of which it is desirable to preserve or enhance”. Also, from time to time authorities are required to review the conservation areas within their districts.
- I.02 Section 71 of the same Act requires local planning authorities to formulate and publish proposals for the preservation and enhancement of conservation areas and to submit them for consideration to a public meeting. Following designation, the local authority in exercising its planning powers, must pay special attention to the desirability of preserving or enhancing the character or appearance of the conservation area (Section 72 of the Act).
- I.03 The Racecourse and Terry’s Factory Conservation Area was designated by the City of York Council in 1975. It is located to the south of the city centre (see Map 1) and the majority of the designated area lies within the Green Belt (see Map 2). The factory was closed

in Autumn 2005 and the Council has prepared a Development Brief to guide the re-development of the site. This is likely to result in significant changes to the Conservation Area and its setting.

- I.04 This document sets out the results of a character assessment of the Conservation Area and is in three sections. The first (Chapter 2.00) is a review of the Conservation Area boundaries. The second section (Chapters 3.00 to 6.00) sets the scene by analysing baseline factors for the entire Conservation Area and identifies three different character areas. Finally, Chapters 7.00 to 9.00 set out a detailed analysis of each of the character areas.



Map 1 Context map

2.00 CONSERVATION AREA BOUNDARY

2.01 One of the requirements of the appraisal was a review of the Conservation Area boundaries to establish if it might be appropriate for there to be any changes. Therefore the extent of the existing designated area has been reviewed and areas around the existing Conservation Area have been studied to ascertain whether any new areas should be included.

Existing boundary

2.02 The existing boundary (see Map 2) runs along the north side of Campleshon Road east from its junction with Knavesmire Road to a point opposite St. Chad's Church (see Fig. 1). It then turns south along the boundary of the former Terry's Factory to a point just north of the factory clock tower. From here it extends along the north side of the historic factory buildings (see Fig. 4) to Bishopthorpe Road. It then follows the eastern and southern boundaries of the factory to Racecourse Road (see Fig. 6).

2.03 From the south-west corner of the factory the Conservation Area boundary runs along the centre of Racecourse Road to the south-east corner of the racecourse enclosure (see Fig. 8). It then turns west, excluding the small pavilion

building to the south of the enclosure. The boundary follows the line of the racecourse enclosure to the southern end of Knavesmire Road.

Possible alterations

2.04 There are no sections within the existing Conservation Area which are considered unsuitable or inappropriate.

2.05 The boundaries along the northern edge of the Conservation Area exclude the houses and other buildings of South Bank (to the north of Campleshon Road - see Fig. 2) and the northern section of the former Terry's Factory from the designated area (see Fig. 5). As these have different characteristics from the Conservation Area, this section of the boundary is entirely appropriate.

2.06 The boundary places the early buildings around the factory clock tower within the designated area but excludes the later structures to the north even where they are attached to the boiler house (see Fig. 3). A short section of the boundary wall of the factory north of the Head Office building on Bishopthorpe Road is excluded from the designated area. This wall is an important feature on the edge of the Conservation Area (see Fig. 6) and



Fig. 1 The boundary along Campleshon Road



Fig. 2 Housing north of Campleshon Road



Fig. 3 Later extension to Boiler House

it would be logical for the whole wall to be within the designated area. However, the wall is considered to be a structure within the curtilage of the listed factory buildings (see Appendix) and is therefore protected by listed building legislation. As a result, an extension to the Conservation Area to include the northern section of the wall would not lead to any additional level of protection. It is therefore recommended that there should be no change to the boundary along this side of the designated area.

2.07 Nun Ings, which lies to the south-east of the present Conservation Area, is of some historic significance as part of the series of water meadows along the river. Also, there are areas of ridge and furrow that is of archaeological interest (see 5.03 below). However, Nun Ings has a very different character from that of the Conservation Area and the section closest to the designated area is partly occupied by a car park for the factory (see Fig. 7). Also, Conservation Area designation would not result in any significant additional protection. It is therefore recommended that the Conservation Area should not be extended to the east.



Fig. 4 Line of existing boundary along the northern elevation of Head Office



Fig. 5 Northern section of factory site



Fig. 6 Existing eastern boundary along Bishopthorpe Road

2.08 The existing Conservation Area boundaries to the south and west relate to the site of the factory and the racecourse enclosure. In view of the visual significance of the open space of Knavesmire, its use for the racecourse, and its importance in views to and from the Conservation Area, there would be some logic in extending the designated area to cover the whole of Knavesmire (see Figs. 9, 10 and 11). Such an extension would include the open spaces on either side of Knavesmire Road, would extend to the Tadcaster Road Conservation Area to the west, might include Knavesmire Wood to the south-west, and possibly extend over part of Middlethorpe Common to the south.

2.09 The inclusion of the whole of the Knavesmire within the designated area would result in a very substantial enlargement of the Conservation Area, consisting almost entirely of open space (unless the buildings defining its northern edges were included). Such an enlargement would not result in any significant additional protection for these areas. It is therefore recommended that the Conservation Area should not be extended to the south and west.

Recommendations

2.10 In view of all the above, it is considered that the existing boundaries of the Conservation Area are entirely appropriate and changes are recommended.



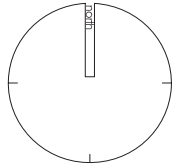
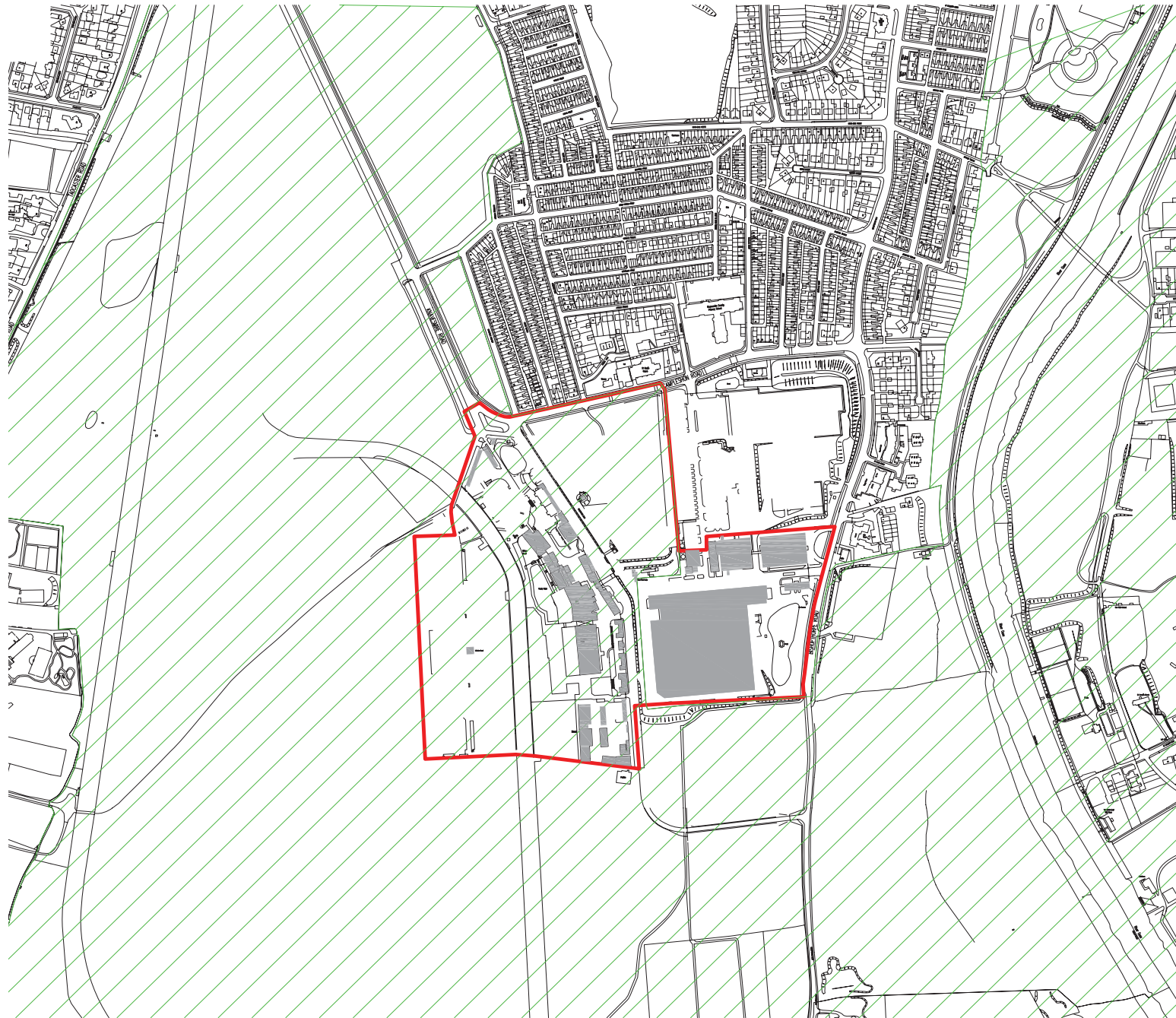
Fig. 7 Factory car park on the east side of Bishopthorpe Road



Fig. 8 South-east corner of racecourse enclosure



Fig. 9 General view of the Knavesmire



— Conservation area boundary

▨ Green Belt

Map 2 - Conservation area boundary

3.00 LOCATION & CONTEXT

3.01 The Racecourse and Terry's Factory Conservation Area lies approximately two kilometres south of the city centre, to the west of the River Ouse (see Map 1). To the north and north-east there are the predominantly residential areas of South Bank and Nunthorpe. To the south-east there are the open spaces of Nun Ings, alongside the river. The open space of the Knavesmire extends around the south and west sides of the designated area (see Figs. 10, 11 and 12).

3.02 Tadcaster Road runs along the ridge to the west of the Knavesmire and was the route of a Roman road into the city. The settlement of Dringhouses straddles this road approximately one kilometre to the west of the Conservation Area. The small settlement of Middlethorpe lies one kilometre to the south.

3.03 The majority of the Conservation Area lies within the City of York Green Belt (see Map. 2). The buildings of both the racecourse enclosure and the factory are major landmarks on the south side of the city, being seen across the open space of the Knavesmire and in association with the large number of trees that surround the various open spaces.



Fig. 10 Conservation Area from west



Fig. 11 Conservation Area from south

4.00 TOPOGRAPHY AND SETTING

- 4.01 The Conservation Area occupies part of the Knavesmire, which is a large flat open area between Tadcaster Road (to the west) and the River Ouse (to the east). The land to the north, west and east (beyond the river) rises gently, giving a slight basin effect, open to the south.
- 4.02 The area is underlain by mixed sands and gravels, with some alluvial deposits, particularly along the river. Nun lngs, to the south-east of the Conservation Area, is one of a sequence of meadows alongside the River Ouse on either side of the city centre. Tadcaster Road follows the line of a glacial moraine (composed of a mix of pebbles, boulders, sands, gravels, and clays), which runs across the Vale of York.
- 4.03 The Knavesmire is predominantly amenity grassland, associated with the racecourse (see Fig. 12). There is some use of Nun lngs for farming and allotment gardens. The York and Selby Path and Cycle Route, which is part of the Trans-Pennine Trail, crosses the area and follows part of the southern boundary of the Conservation Area.
- 4.04 The open spaces of the Knavesmire are partly defined by trees. Knavesmire



Fig. 12 Aerial view of the Conservation Area and its setting

Wood to the south-west and the large number of mature trees on the higher ground within the grounds of properties on the east side of Tadcaster Road (see Fig. 9) are of particular importance in defining the edge of the main open area. There is also a very significant avenue of mature trees along Knavesmire Road. The north-east edges of the Knavesmire are defined by the houses of South Bank and the buildings of the racecourse.

4.05 There are few trees elsewhere in the vicinity. Within the Conservation Area there is a garden area in the south-east corner of the factory site, which is surrounded by mature trees, and there are significant belts of trees marking the south and west boundaries of the factory (see Figs. 12 and 14). To the south-east of the designated area there are hedgerows along Bishopthorpe Road and the remnants of hedgerows on some of the field boundaries, a few of which contain mature hedgerow trees.

4.06 The Conservation Area has a predominantly open setting, with few buildings visible to the west, south or east (see Fig. 12). Dringhouses to the west is largely concealed by the mature trees on the east side of Tadcaster Road.

The small group of buildings that constitute Middlethorpe are some distance from the Conservation Area and are partly screened by trees. There are a few houses on the east side of Bishopthorpe Road to the north of Nun Ings (see Fig. 13). The northern section of the former Terry's Factory, which is predominantly open but also contains a number of modern industrial buildings, provides the setting of the designated area to the north-east. The suburb of South Bank, which predominantly consists of terraced housing (see Fig. 2), has some impact to the north but this is limited as it only directly abuts the designated area alongside Park Pavilion Field and this is limited.



Fig. 13 Residential development on the east side of Bishopthorpe Road



Fig. 14 Open space to the south of the Conservation Area

5.03 By the medieval period there was some arable farming in the area (most probably linked to Middlethorpe), as evidenced by areas of ridge and furrow at the north and south ends of Nun Ings. However, the Knavesmire appears to have been predominantly used as common pasture for the manor of Dringhouses (which was in the ownership of the Archbishop of York) and the freemen of Micklegate Ward (see the name Micklegate Stray on Maps 3 and 4). It was also the location of a public gallows, the site of which is marked by a stone engraved 'Tyburn' alongside Tadcaster Road.

5.04 There are records of horse racing in York from the sixteenth century; the annual race meeting was moved to the Knavesmire from Clifton Ings in 1731. Racing became part of the social calendar and in 1754 the first grandstand was erected. This was designed by John Carr, then a local stonemason, who was to become one of the most successful provincial architects of the period, in part because of this commission. Part of this grandstand survives as part of the Guinness Bar (see Appendix).

5.05 Horse racing was important to the social and economic development of the city as



Map 4 1909 Ordnance Survey

the annual Race Week was the focus for aristocratic and gentry life during the eighteenth century. Its significance declined during the first half of the nineteenth century, but York re-emerged in the twentieth century as one of the major racecourses in the country.

5.06 The growth of the racecourse has been almost continuous and a number of its structures are included on the List of Buildings of Special Architectural or Historic Interest (see Appendix). The list description for the County Stand indicates that it was erected about 1840 but it is not shown on the Ordnance Survey Map of 1853 (see Map 3), which only shows the original, John Carr, grandstand (see Fig. 15 and 5.04 above). Various other stands and ancillary structures can be identified by 1909 (see Figs. 16, 17 and 18, and Map 4).

5.07 There appears to have been a major development of the racecourse in the 1920s (see Map 5), which included the formation of Racecourse Road, the construction of the enclosure walls, and the erection of a range of small ancillary buildings, plus the erection of additional stands and an indicator board and clock tower. Three large stands (the Melrose,



Fig. 15 Early Photo of John Carr's grandstand



Fig. 16 View of the stands circa 1909



Fig. 17 View of the stands circa 1909

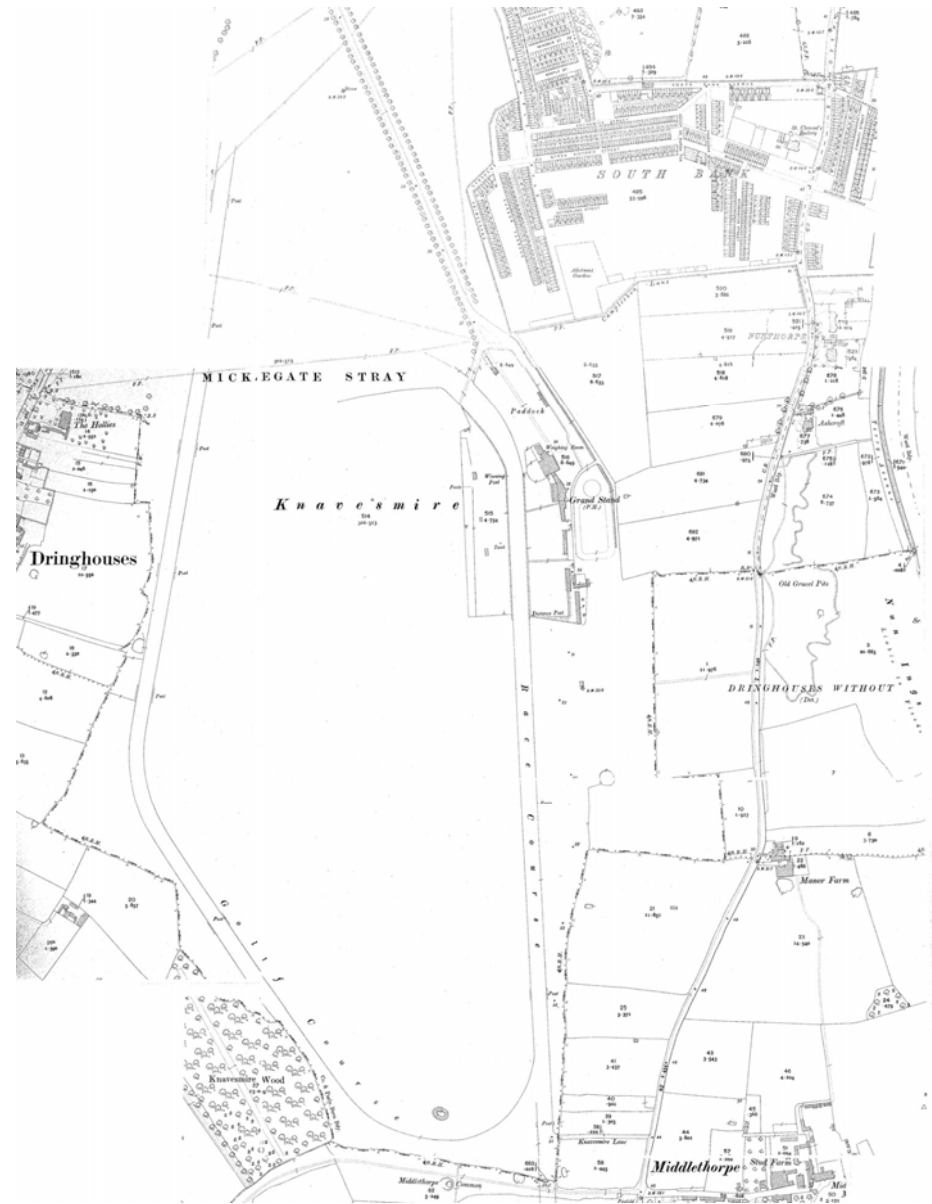


Fig. 18 View of the County Stand circa 1909

Ebor and Knavesmire Stands) were erected at the end of the twentieth century (see Fig. 21).

5.08 The area remained relatively open, apart from the settlements of Dringhouses, Nunthorpe and Middlethorpe, until the late nineteenth century when the suburb of South Bank began to develop (see Maps 4 and 5). By the 1930s this growth had extended south to Campleshon Road.

5.09 Terry's of York began in 1767 as a confectionary business owned by Bayldon and Berry on a site near Bootham Bar. Joseph Terry trained as an apothecary and joined the firm as he was connected by marriage to Berry. By 1830 Joseph was the sole owner and gradually developed the chocolate side of the business. He was succeeded by his sons. The company developed until it became one the major employers in the city; its products and name being inextricably linked to York. New factory premises were built at Clementhorpe in the 1860s to provide better transport links, but continued expansion led to the need for new premises, which led to the Bishopthorpe Works, erected in the 1920s to the design of architects J.G.



Map 5 | 1931 Ordnance Survey

Davies and L.E. Wade. These works were clearly designed to make a bold statement about the status of the company (see Fig. 19).

5.10 The early buildings of the new works (see Map 5) consisted of a head office facing Bishopthorpe Road, time office, five-storey factory, north-light shed (which is slightly earlier than and was originally separate from the five-storey factory), boiler and transformer house, with the boiler flue designed as a clock tower, and a liquor store. All these structures are now included on the List of Buildings of Special Architectural or Historic Interest (see Appendix A). Later buildings to the north were of lesser architectural significance and included a jam factory, which was used as an underground shelter during the Second World War.

5.11 Developments during the second half of the twentieth century included extensions to the north-light shed, the construction of an additional factory building with bridge links to the five-storey factory and the head office, and various buildings to the north (outside the Conservation Area), some of which have subsequently been demolished.



Fig. 19 View of the Bishopthorpe Road entrance circa 1940



Fig. 20 Aerial view into the Conservation Area circa 1956

6.00 GENERAL CHARACTER

6.01 This section considers the general character of the whole of the Conservation Area, as distinct from the analysis of the three character areas that have been identified (see 6.07 below).

6.02 The key characteristic of the Conservation Area is that of two groups of substantial buildings within a landscape setting. This open 'parkland' setting, provided by the open spaces outside the designated area, is in sharp contrast to the buildings and is of importance to the character and appearance of the designated area. Also, the many mature trees around and within the Knavesmire provide an important foil to the buildings.

6.03 Views across the open landscape are also of major significance. The stands of the racecourse are inevitably designed to exploit the views out across the Knavesmire. In contrast, the former Terry's Factory has an inward-looking character, being a generally enclosed site, with walls and trees around the perimeter. This limits views out of the factory site at ground level, although there are medium and long distance views from the upper levels of the main factory building and the clock tower.

6.04 Views towards the Conservation Area are of greater significance than the views out. The large stands of the racecourse are very prominent in views across the Knavesmire from the west (see Fig. 21) and are the focal point in views from Tadcaster Road north of the centre of Dringhouses. The upper levels of the main building and clock tower of the factory dominate the whole of the surrounding area, being seen in views from Campleshon Road, Bishopthorpe Road, the racecourse and Knavesmire Road. They are also major landmarks on the wider York skyline, being visible from the tower of the Minster, from sections of the city walls, from many of the open spaces within the city (such as Rowntree Park), from the A64, and in many other views from the south.

6.05 At present, except on race days, the character of the Conservation Area is of a quiet location, with limited activity. Although the racecourse is used for conferences and other events these generally make little impact. The historic and intended role of the factory was as one of the city's major companies, employing a large number of people, many of whom lived in the surrounding residential areas, such as South Bank.



Fig. 21 The Ebor and Knavesmire Stands



Fig. 22 Pedestrian activity along Racecourse Road on race days



Fig. 23 The open ground known as Park Pavilion Field

Following its closure the factory is (currently) no longer a centre of activity (see 9.03 below); this will change following the re-development of the site (see 1.03 above). The wider area is also relatively quiet, with only casual and occasional recreational use of the Knavesmire, although both Campleshon Road and Bishopthorpe Road carry significant volumes of traffic.

6.06 On race days the character is very different, with large numbers of people and vehicles (including helicopters) converging on the racecourse, car parking on Park Pavilion Field and elsewhere around the designated area, and bursts of noise at the conclusion of each race (see Figs. 22, 23, 26 and 27).

6.07 The racecourse has a wide variety of buildings, a number of which are included on the List of Buildings of Special Architectural or Historic Interest (see Appendix and Map 8). However, the early twentieth century ancillary buildings, which line the west side of Racecourse Road, have a consistency of scale, form, style and materials that provides the entrance to the racecourse with a very distinctive character and appearance.

6.08 The buildings of the former Terry's Factory that are within the Conservation Area generally have a consistency of architectural form and style, although the scale varies considerably (see Figs. 24 and 25). The factory was developed in the 1920s and the original buildings are all constructed of red brick with buff sandstone dressings. They are also all in a consistent Baroque Revival style and have recently been added to the List of Buildings of Special Architectural or Historic Interest (see Appendix and Map 8).

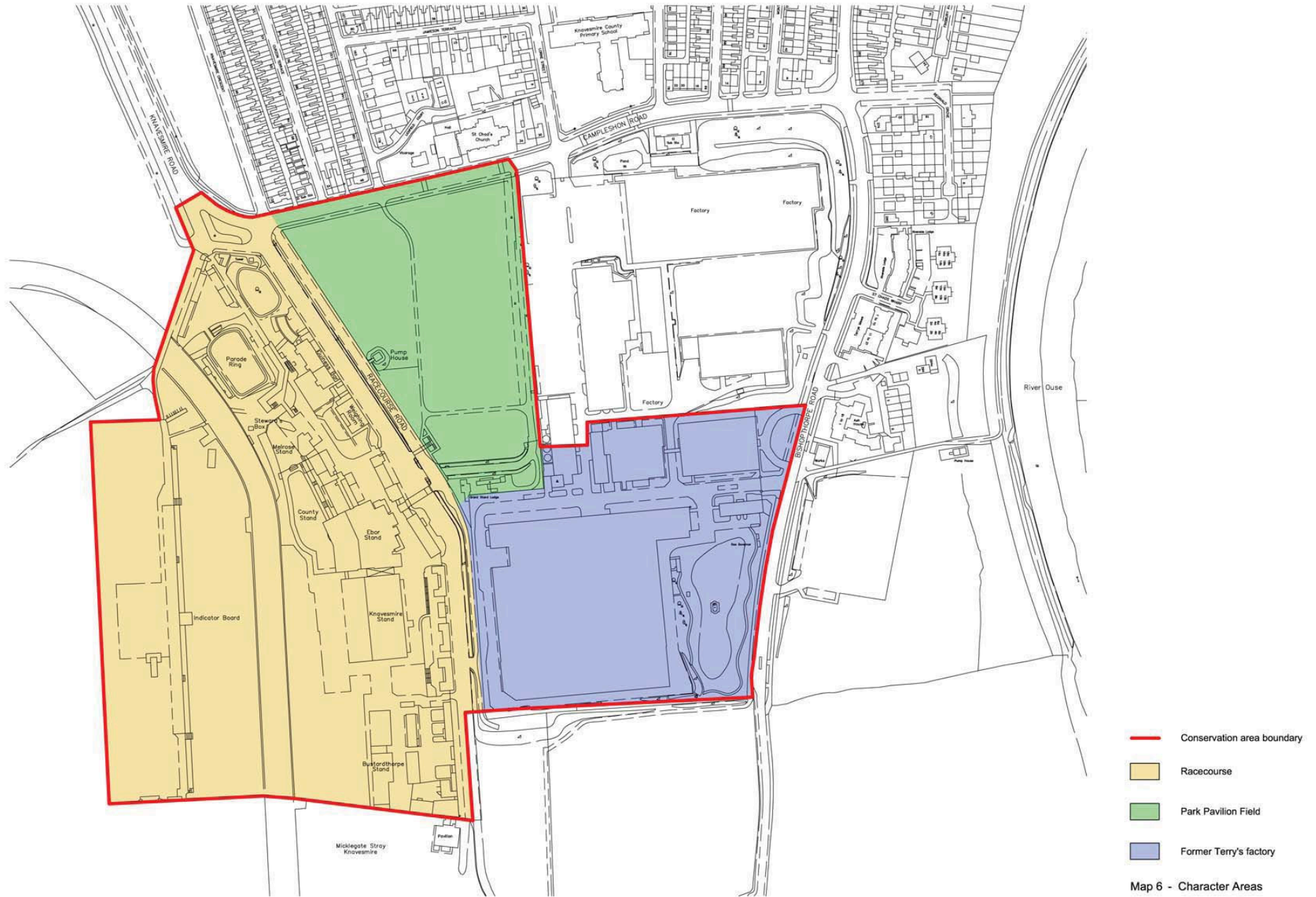
6.09 As already indicated (see 6.02 above) the Conservation Area consists of two groups of buildings with very different characters; the racecourse enclosure and the former Terry's Factory. In addition, the Park Pavilion Field to the south-east of the junction of Campleshon Road and Racecourse Road also has its own distinct character. As a result, for the purposes of more detailed analysis, the Conservation Area has been divided into three distinct character areas (see Map 6).



Fig. 24 View of the frontage buildings along Bishopthorpe Road



Fig. 25 View of the five-storey factory



7.00 THE RACECOURSE ENCLOSURE

7.01 This character area (see Map 6) consists of the racecourse enclosure and Racecourse Road. The racecourse loops around the edges of the Knavesmire and only a short section is therefore within the Conservation Area.

7.02 The special interest of this character area is the result of a number of different factors: the size and variety of the stands; their relationship to the open space of the Knavesmire; the consistency of the architectural treatment of the small-scale buildings along the east wall of the enclosure; and the 'party' or 'festive' atmosphere generated on race days.

Uses

7.03 The racecourse enclosure is one of the major leisure complexes within York. It is a hive of activity on race days, attracting very large numbers of people (see Figs. 26 and 27). This results in many of the adjacent areas (most of which are outside the Conservation Area) being used for car and coach parking. At other times the buildings are used for a variety of functions but these do not result in the same level of activity. As a result, for much of the year the racecourse enclosure is relatively quiet.



Fig. 26 View of the crowds on race day



Fig. 27 Crowds on race days

Qualities of buildings

- 7.04 The buildings within the racecourse enclosure are varied in both their size and architectural qualities. The various stands are arranged in a gentle curve that follows the alignment of the course (see Fig. 34). These date from different periods and vary in both scale and style; the earliest are listed (see Appendix and Map 8).
- 7.05 Part of the original grandstand, designed by John Carr in 1754, has been reconstructed and is now part of the Guinness Bar (see Figs. 28 and 29 and Appendix) and is therefore no longer used as a stand. The earliest stand in use is therefore the original section of the County Stand, which dates from the mid-nineteenth century. It has two tiers of arcades supported on slender twisted columns (see Appendix). The original lower level, which can be seen in Figs. 16 and 18, has now been concealed by stepped terraces. The County Stand has been extended and altered on a number of occasions and its northern end, which was constructed around 1890 and was at one time used as the Press Stand, is listed in its own right (see Appendix).
- 7.06 The later stands have a very different character. The Bustardthorpe Stand, at

the southern end of the enclosure, dates from the early twentieth century and is a concrete structure with an interesting architectural treatment of panels and fins on its rear (east) elevation (see Fig. 33). In contrast, the recent stands (Melrose, Ebor and Knavesmire) are substantial structures, making a major impact upon the surrounding area. The brickwork and pitched roof of the Melrose Stand makes references to the smaller scale buildings of the 1920s (see 7.06 and 7.07 below), although its size makes this design approach inappropriate (see Figs. 34 and 38). The Ebor and Knavesmire Stands adopt a bold contemporary approach (see Figs. 21, 31 and 34).

- 7.07 The indicator board and clock tower (now abandoned), which stands in an isolated position within the loop of the racecourse, is listed (see Fig. 30 and Appendix). It was designed by Walter Brierley, a noted York architect of the early twentieth century, who may also have designed the small-scale buildings of this period along the eastern edge of the racecourse enclosure (see Figs. 22, 31 and 37). These buildings have a very different character from the stands. These are generally single storey in height and are in a simple vernacular style, with



Fig. 28 The Guinness Bar from north-west



Fig. 29 Rear of the Guinness Bar



Fig. 30 The abandoned Indicator Board

some use of classical details. There are a number of recent buildings along this eastern edge of the enclosure (fronting Racecourse Road), the majority of which have been designed in the same idiom.

7.08 As noted in 7.04 above, part of the mid-eighteenth century grandstand of the racecourse has been reconstructed and now forms part of the Guinness Bar. A series of lightweight canopies have been added to the front (west) elevation of the earlier structure, which obscure its architectural qualities (see Fig. 28).

7.09 Four of the structures within the racecourse enclosure are listed (see Appendix and Map 8). Many other structures are considered to make a positive contribution to the Conservation Area (see Map 8), including all the stands, with the exception of the Melrose Stand, and most of the buildings along the eastern and northern edge of the enclosure.

Other structures

7.10 The boundary wall to the north, east and south sides of the racecourse enclosure appears to date from the 1920s (see 5.05 above), although some sections have been rebuilt. The wall is generally simple in

appearance but there is an impressive gateway, flanked by elaborate stone piers at the northern end of the enclosure (see Fig. 32). This wall is of significance because of its role in defining the racecourse enclosure and it is therefore considered that it makes a positive contribution to the Conservation Area.

7.11 The pedestrian bridge across Racecourse Road also appears to date from the 1920s (see Fig. 35 and Map 5). Its white-painted concrete provides a sharp contrast to the predominant colours and materials of the other structures along the road, however, its style relates to that of the Bustardthorpe Stand (see 7.06 above). The bridge is typical of its period and is a significant feature of this area. It is therefore considered that this also makes a positive contribution to the Conservation Area.

Spaces and views

7.12 There are four key spaces within this part of the Conservation Area: the wide open area to the west of the stands opening out onto the Knavesmire; the relatively narrow area to the east of the main stands (excluding the area to the east of the Bustardthorpe Stand); the Parade Ring and its surroundings; and Racecourse Road.



Fig. 31 Racecourse enclosure from south-east



Fig. 32 Entrance gates at northern end of the racecourse enclosure



Fig. 33 Bustardthorpe Stand from the south-east

7.13 The area to the west of the main stands is the major space of the enclosure as it is overlooked by all the stands and is packed with spectators on race days (see Fig. 26). The ground surface is predominantly grass. To the east it is enclosed by the stands but it is open on its other three sides, apart from the barriers alongside the track. On race days a variety of temporary structures are erected around this space. There are major views to the west, both from ground level and from the stands. There are also key views across the Knavesmire towards the main stands, which are major landmarks.

7.14 The area to the east of the main stands is effectively the initial gathering space for race goers as it is the transition between the various entrance buildings and turnstiles along Racecourse Road and the main spectator areas (see Fig. 37). The ground is largely hard surfaced, although there are limited areas of grass and some key trees in the southern section (see Map 9). It is an enclosed area, with small-scale buildings to the east and the bulk of the stands to the west. As a result there are no views into or out of this space and no views of any significance within it.



Fig. 34 The stands from the race track



Fig. 35 Bridge link across Racecourse Road

7.15 The Parade Ring to the north of the main stands is of special significance. The area around it is predominantly surfaced with grass. To the north and east the space is surrounded by low-key buildings, but to the south there is the substantial bulk of the Melrose Stand. The space is open to the racecourse to the west, allowing views out to the Knavesmire. There are no significant views into this space from the wider area.

7.16 Racecourse Road extends along the east side of the racecourse enclosure (see Fig. 35). It is a curved linear space and, as a result of the higher ground and retaining walls to the east, there is a sense of being in a cutting. The trees along the west side of the factory site provide a greater sense of enclosure for one section of the road. The wall and buildings of the racecourse enclosure extend along its west side. The bridge punctuates the space but the many entrances to the enclosure mean that there is no single focal point. Views along the road are generally limited because of its curving nature. There are oblique views up to the main stands and to the clock tower and five-storey building of the factory.

Soft landscaping

7.17 There are few trees within this section of the Conservation Area. At the northern end of the racecourse enclosure, inside the gateway (see Fig. 32) there is a small group of trees and there are also a few trees within the designated area outside this gateway. The trees that give the southern end of Knavesmire Road its sylvan appearance lie outside the Conservation Area. Similarly, the trees on the higher ground to the east of Racecourse Road are mainly within the former Terry's Factory.

Neutral and negative factors

7.18 There are no negative factors within this part of the Conservation Area. Although the light-weight canopies of the Guinness Bar detract from the listed building (see 7.08 above), they contribute to the 'party' or 'festive' atmosphere of race meetings and are therefore important to the character of the area. The Melrose Stand is considered to be a neutral factor as a result of its uninspired architectural treatment (see 7.06 above). A number of the ancillary structures are also neutral factors as a result of their utilitarian appearance (see Map 10).



Fig. 36 The Parade Ring

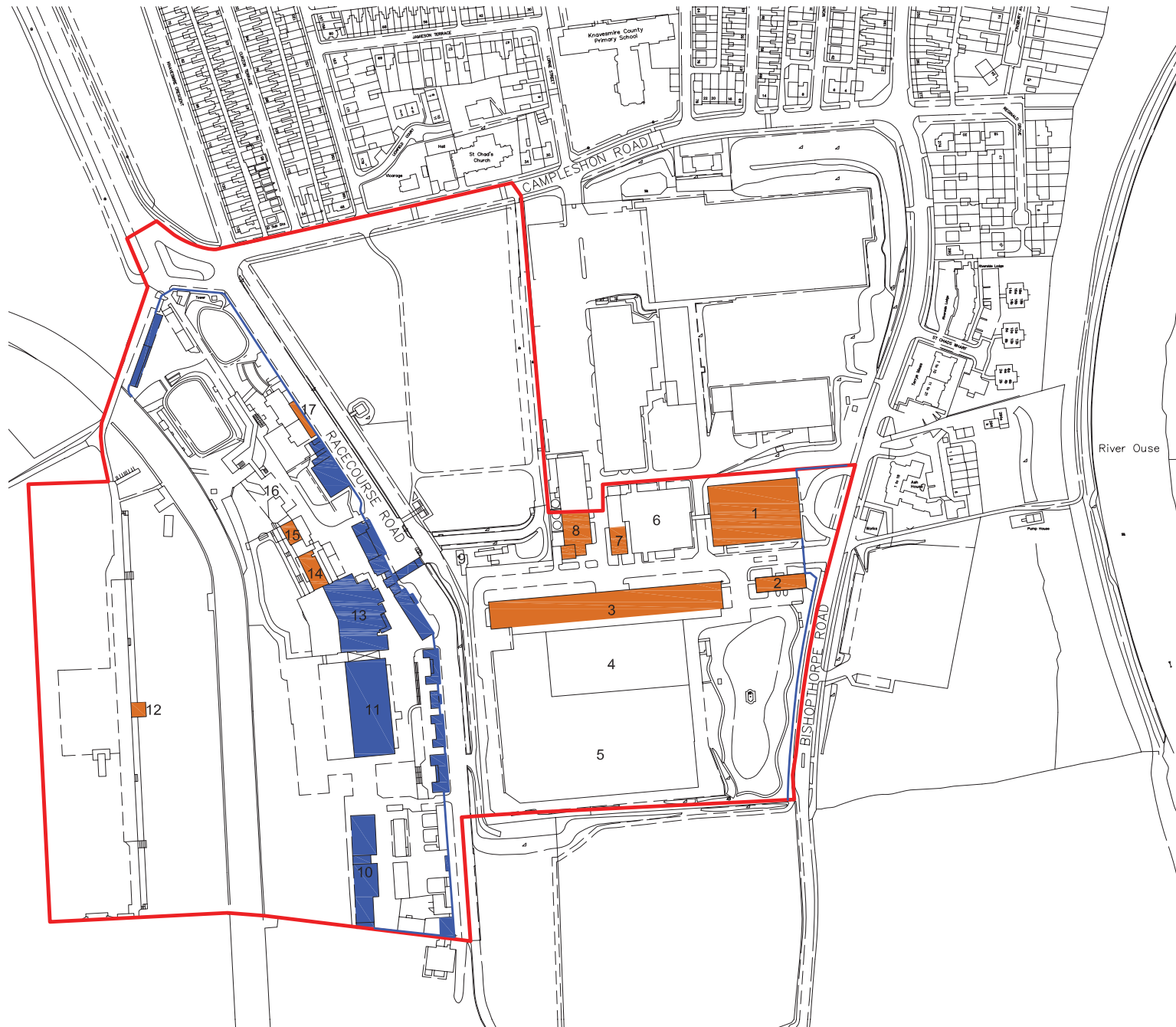


Fig. 37 Space to the east of the stands



Fig. 38 The Melrose Stand

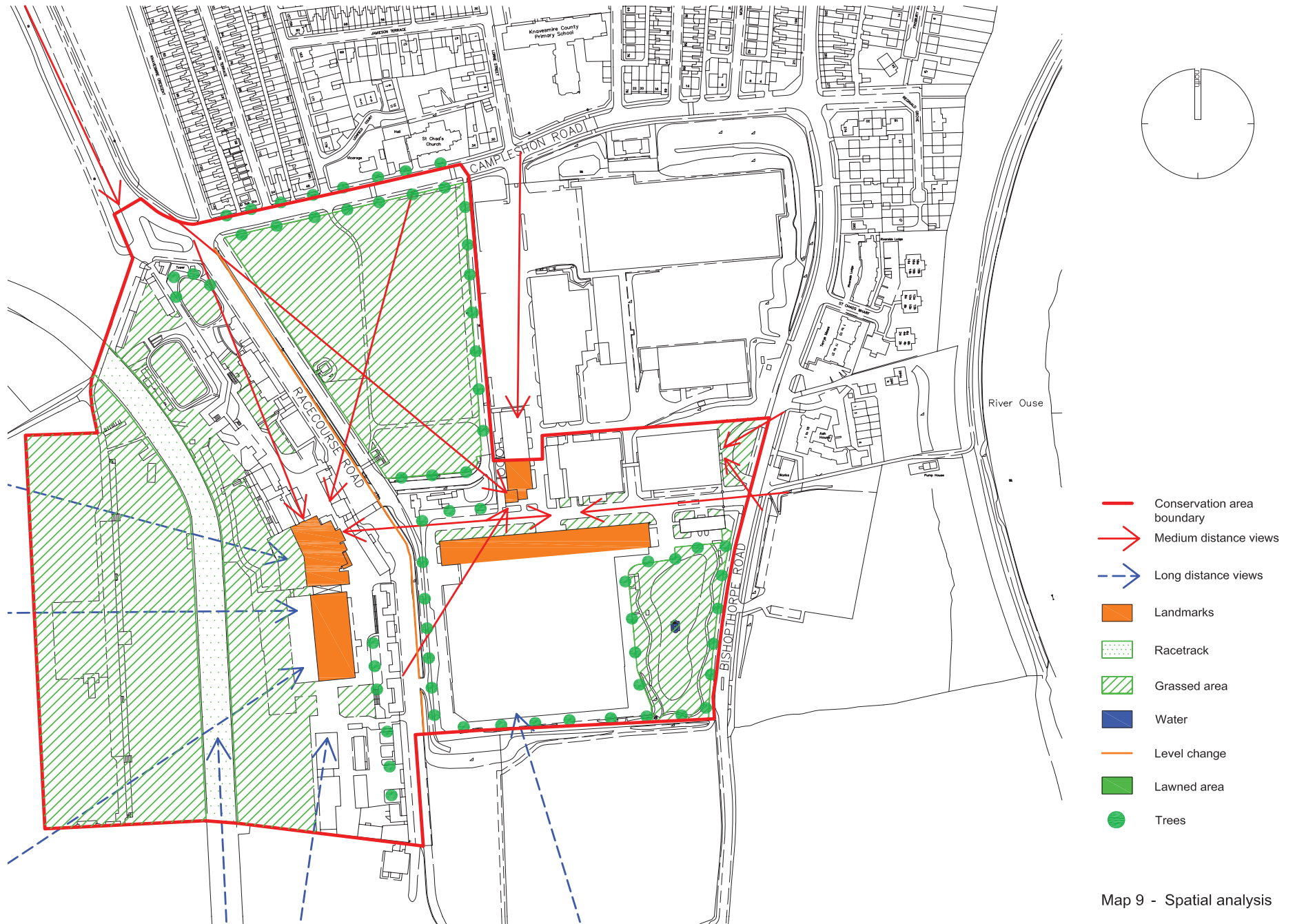




- 1 - Head office
- 2 - Time office
- 3 - Factory
- 4 - North light shed
- 5 - Modern elements
- 6 - 1970s block
- 7 - Liquor store
- 8 - Clock tower, Boiler & Transformer house
- 9 - Grand Stand Lodge
- 10 - Bustardthorpe Stand
- 11 - Knavesmire Stand
- 12 - Indicator Board
- 13 - Ebor Stand
- 14 - County Stand
- 15 - County Stand extension
- 16 - Melrose Stand
- 17 - Guinness Bar

- Conservation area boundary
- Listed buildings
- Buildings which make a positive contribution

Map 8 - Buildings or structures of significance





8.00 PARK PAVILION FIELD

8.01 This character area (see Map 6) predominantly consists of the open space to the west of Racecourse Road and north of the former Terry's Factory. It also includes Grand Stand Lodge, which is located to the south of the open space adjacent to the factory boundary. This area sits at a higher level than Racecourse Road.

8.02 The special interest of this character area is the nature of the open, grassed space, fringed by trees on three sides, which allows views of the main stands of the racecourse and the major buildings of the factory (see Fig. 40).

Uses

8.03 The open space is used for a variety of functions related to the racecourse. On race days it operates as a car park (see Fig. 39) and at the southern end a tented village is erected to provide additional ancillary space. When it is intensively used on race days this space has a very different character compared to its emptiness for much of the rest of the year. A strip along the southern edge of the area is occupied by the vehicular ramp up from Racecourse Road and Grand Stand Lodge and its curtilage.



Fig. 39 View north across Park Pavilion Field



Fig. 40 View south-east across Park Pavilion Field

Structures

8.04 There are few buildings or structures within this part of the Conservation Area. On the western edge of the open space there is a small brick pump house with a flat roof (see Fig. 41). To the north-east of the pedestrian bridge there are two links between the open space and lower level of Racecourse Road; one pedestrian, the other vehicular. The pedestrian link consists of a double flight of steps below a pergola and a short underpass (see Fig. 42). The vehicular route is a ramp, which has a short bridge at its western end to carry the raised route along the east side of the road. Grand Stand Lodge, at the southern end of this section of the Conservation Area, is a medium-size house typical of the mid-twentieth century (see Fig. 43). It is constructed of buff bricks and with a shallow-pitched, concrete-tiled roof.

Spaces and views

8.05 The majority of this part of the Conservation Area consists of an open space of maintained grass. This is semi-enclosed on the north, east and south sides by lines of trees (see Fig. 39). Views are possible through these trees to the houses on the north side of Campleshon Road, to the modern buildings of the

former Terry's Factory to the east, and to the upper levels the original factory buildings to the south (see Fig. 40). The western side of the space is open, allowing views across Racecourse Road to the main stands. There are views into this open space from Campleshon Road and important long-distance views from the north across the space to the landmark buildings of the factory. The curtilage of Grand Stand Lodge is a small, domestic enclave at the southern end of the space.

Soft landscaping

8.06 The maintained grass of the open space and the surrounding trees are important in establishing the character of this part of the Conservation Area.

Neutral and negative factors

8.07 There are no negative factors within this section of the Conservation Area. It is considered that the pump house, the pedestrian steps and Grand Stand Lodge are all neutral factors (see Map 10).



Fig. 41 Pump house



Fig. 42 Steps across Racecourse Road



Fig. 43 Grand Stand Lodge

9.00 FORMER TERRY'S FACTORY

9.01 This character area (see Map 6) consists of the southern section of the former Terry's Factory (the northern section is not within the Conservation Area) and includes all the early buildings of the complex .

9.02 The special interest of this character area is the result of the consistent, high-quality Baroque Revival design approach adopted for the early factory buildings (see Figs. 44, 46, 47, 48 and 51), the layout of the factory buildings on a rectilinear grid arranged around two axis (one through each of the entrance gates), the generally inward-looking nature of the site, and its former role as a major centre of employment.

Uses

9.03 The factory was a major industrial complex (see Map 7) but has now closed and the buildings are empty. As a result this part of the Conservation Area is currently very quiet, with few people and little vehicle movement. However, this is very different from its intended and historic character (or its character when the Conservation Area was designated), which must have generally been a bustling and noisy environment. The re-development of the site (see I.03 above)



Fig. 44 The Bishopthorpe entrance



Fig. 45 View looking north-west within factory grounds

will re-establish something of the site's original character.

Qualities of buildings

9.04 The character and appearance of this part of the Conservation Area is largely determined by the layout, scale and qualities of the early buildings of the factory (see Figs. 44, 46, 47, 48 and 51). These were laid out in response to two axes: one east-west through the main entrance on Bishopthorpe Road (see Fig. 19); the other north-south from the Campleshon Road entrance. As a result the buildings form a very strong grid pattern on the site, which has also been followed by the later buildings both within and outside the designated area.

9.05 The early buildings of the complex were all designed on a large scale, in particular the five-storey factory. These buildings are all in a Baroque Revival style, with external walls of red brick and extensive use of ashlar sandstone dressings. The head office has an ornate front elevation with its own drive approach from Bishopthorpe Road (see Fig. 51). The other buildings have less elaborate detailing but are still designed to impress. The window openings are vertically proportioned and contain sashes sub-

divided into small panes. All of these buildings, apart from the north-light shed, have flat roofs concealed by parapets. The consistency of materials and detailing used on these buildings provides a strong and uniform character to this part of the Conservation Area.

9.06 These early buildings are generally unaltered; the main change being the partial demolition and subsequent extensions to the north-light shed. In addition, all the windows on the time office have been replaced.

9.07 The extensions to the north-light shed (which is itself the least significant of the early factory buildings) are poorly detailed being typical industrial structures of their period, although they generally make extensive use of red bricks, which is the predominant material of the early buildings (see Figs. 45, 55 and 56). The building of the 1970s, to the north of the main access road, and its two bridge links are very different, having extensive areas of white concrete cladding and glazing (see Fig. 57).

9.08 The principal buildings in this part of the Conservation Area (the head office, time office, five-storey factory, boiler and



Fig. 46 Simple palette of materials



Fig. 47 The Time Office



Fig. 48 The Liquor Store

transformer house, clock tower, and liquor store) are all listed (see Map 8 and Appendix). None of the other buildings on the factory site (including the remaining section of the north-light shed) are considered to make a positive contribution to the Conservation Area (see Map 8).

Other structures

9.09 The eastern boundary of the factory, along Bishopthorpe Road and on either side of the forecourt of the head office building, appears to be of two phases (see Fig. 49). The early sections, which were contemporary with the original factory buildings, comprise a brick and stone plinth that would have supported railings between tall brick piers. Subsequently the piers have been raised in height, the railings removed and replaced with a brick panel. This wall is within the curtilage of the listed factory buildings but is also of significance because of its role in defining the edge of the Conservation Area along Bishopthorpe Road. The gate piers at the main entrance, to the south of the head office, appear to be largely unaltered, although the original gates have been replaced (see Fig. 44). It is considered that the wall, together with the gate piers, makes a positive contribution to the

Conservation Area (see Map 8).

9.10 There are a variety of other minor structures and elements within the factory site that are reminders of its industrial function. These are of interest but are not considered to make a significant contribution to the character of the Conservation Area.

Spaces and views

9.11 There are three key spaces within this part of the Conservation Area: the forecourt to the head office; the main access road; and the garden area in the south-east corner of the site. Other spaces are simply the roads and spaces around and between the buildings.

9.12 The forecourt to the head office opens off Bishopthorpe Road (see Fig. 51). To the west there is the elaborate main elevation of the head office, while to the north and south it is enclosed by the brick boundary walls (see 9.05 above). This space has a simple open character, consisting of the sweep of the access drive with lawns on both sides. There are important views across this space to the main elevation of the head office and the main gates of the factory.

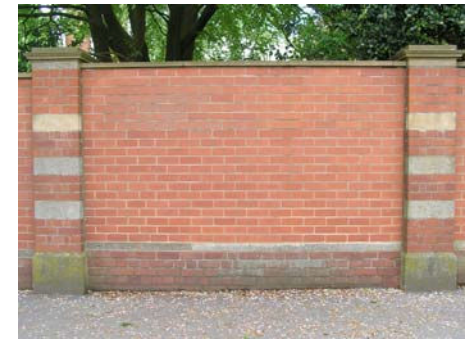


Fig. 49 Detail of the boundary wall along Bishopthorpe road



Fig. 50 Metal ventilator



Fig. 51 Forecourt to the Head Offices

9.13 The main access road runs from the main gates into the complex off Bishopthorpe Road westwards across the full extent of the factory site (see Fig. 44). It is flanked by buildings, which are set back behind narrow areas of lawn with some shrubs planting and a few trees. There are strong east-west axial views along this space, particularly to the clock tower and to the Ebor Stand of the racecourse. There are also views out to the north and south between the buildings.

9.14 The garden area in the south-east corner of the site (see Figs. 25 and 52), which was presumably designed for the use of the factory workers, has a very different character and appearance from the predominantly hard nature of the majority of this section of the Conservation Area. It is a soft green oasis, where wildlife is both audible and visible. It consists of an irregular shaped lawn surrounded by trees and shrubs. At the centre of the space is a formal pond and fountain. This is an enclosed area with few views in or out.

Soft landscaping

9.15 This part of the Conservation Area has important belts of mature trees along its western, southern and eastern



Fig. 52 Garden area with water feature



Fig. 55 Modern extension to five-storey factory



Fig. 53 Boiler and Transformer house



Fig. 56 Modern extension to north light shed



Fig. 54 Industrial paraphernalia



Fig. 57 1970s block and bridge link

boundaries, and along the west end of its northern boundary (see Map 9). These trees provide the setting to the various buildings and screen the factory site from the surrounding areas. There are also important belts of trees to the north and west of the garden area in the south-east corner of the site, which screen it from the adjacent buildings. There are also a few trees and shrubs in the lawn areas on either side of the main access road.

Neutral and negative factors

- 9.16 The only neutral factor within this part of the Conservation Area is the surviving section of the north-light shed (see 9.06 above). There are, however, a large number of negative factors as noted below and identified on Map 10.
- 9.17 Due to the ever changing requirements for the manufacture of chocolate products, many of the buildings of architectural value have suffered from unsympathetic alterations. Windows openings have been blocked or altered, and the five-storey factory has several inappropriate extensions to its southern elevation (see Figs. 55). Also, the modern industrial buildings to the south and west of the remaining section of the north-light shed are detrimental to the appearance of

the area because of their form and materials (see Figs. 45 and 56).

- 9.18 The 1970s building on the north side of the main access road, together with its two bridge links, has a detrimental impact on the appearance of the area, because of its uncharacteristic materials, horizontal emphasis and expressed frame (see Fig. 57). The bridge link to the five-storey factory is particularly intrusive as it disrupts the view west from the main gates (see Fig. 44).
- 9.19 The confusion of signage on the gate piers of the main entrance from Bishopthorpe Road detracts from the quality of these important structures (see Fig. 58).
- 9.20 The two tall metal storage tanks in the belt of trees to the west of the garden area also detract from the character and appearance of the area.



Fig. 58 Inappropriate signage at main entrance

APPENDIX

LISTED BUILDING DESCRIPTIONS

Guinness Bar in the Old Grandstand
York Racecourse

Grade II*

Grandstand, now bar. 1755; demolished and partly reconstructed c1920. By John Carr. Orange brick in English bond with dressings of ashlar sandstone: roof not visible.

EXTERIOR: 1-storey 9-bay front on moulded stone plinth: centre and end bays are faced with rusticated stone and break forward; centre bay is pedimented. Bays are arcaded, the end and centre ones with round arches of radiating voussoirs, elsewhere rubbed brick arches with stone impost band. Each arch has a C20 canopy over. Moulded stone eaves cornice beneath balustraded parapet of bulbous balusters with plain stone piers and moulded coping.

Rear: 1 storey 9 bays, on brick plinth. Outer and centre bays are blind; elsewhere blind arcaded beneath stepped brick cornice and plain parapet with flat stone coping.

INTERIOR: not inspected.

HISTORICAL NOTE: this is all that remains of one of the most impressive C18 Grandstands in England.

(Murray H, Riddick S & Green R: York through the Eyes of the Artist: York City Art Gallery: 1990 - : 24; RCHME; City of York: London: 1972 - : 50).



County Stand
York Racecourse

Grade II

Racecourse stand. C1840 with early C20 alterations and extensions. Cast-iron and timber with boarded awning on iron trusses: slate hipped roof with wide eaves on timber brackets.

EXTERIOR: 2-storey 11-bay front arcaded in two tiers of segmental arches on twisted columns with floral capitals on tall pedestals and with spandrels filled with openwork arabesques. Lower tier is open at the front: 8 bays have curved awning with scalloped valance above. Upper tier has ornate balustrade of cast-iron openwork panels.

INTERIOR: lower tier stepped and fitted with wooden bench seats. Rear partition wall incorporates glazed and panelled doors and coat pegs. Behind partition wall, close string staircase has substantial turned balusters and shaped handrail on square sunk-panel newels with ball and pedestal finials. Both tiers have moulded cornices.



Press stand
York Racecourse

Grade II

Racecourse stand, now press stand. C1890 with later alterations. Rear of orange brick in English garden wall bond banded in white brick and with stone copings: at front, cast-iron columns on stone pedestals support shallow hipped slate roof on shaped timber brackets which has cast-iron guttering and terracotta finials.

EXTERIOR: 2-tiered front of 4 bays defined by columns with foliate capitals on high pedestals. Lower tier is terraced. Upper level is partitioned off with glazed and boarded screen incorporating glazed and flush panelled doors, one sliding. Rear: 3 storeys 5 bays, bays defined by pilasters with openings set between them in recessed vertical panels. Ground floor altered for tote windows. First floor has 4-pane sash windows with stone sills and stone keyed flat arches of rubbed brick: fifth window at right end altered. Second floor windows are 1-pane lights with stone sills and top hung shutters. Bands of cogged brick over ground floor openings and at eaves level. INTERIOR: not inspected.



Indicator Board and Clock Tower
York Racecourse

Grade II

Indicator board and clock tower. 1922. Designed by Walter Brierley. Steel framed, wooden and re-constituted stone with a hipped slate roof.

EXTERIOR: 3 storeys plus attic. Square plan.

Ground floor has square corner piers and between on 3 sides 2 Doric columns in antis, all of re-constituted stone. First and second floors are divided into 3 panels on each of the 3 front faces, these panels are each divided into 6 spaces for name boards. In the centre of each of these faces is a seventh panel in a central square dormer with a flat lead roof. The main front has attached frames to each side for further name boards. The square hipped roof is topped by a square lantern with a circular clock face to each side, and a moulded square lead dome topped with a finial. The 7 day clock was made by GJF Newey of York and installed in 1923.

INTERIOR: retains all its original structure and fittings.



Head Office
Terry's of York

Grade II

Head office building for Terry's of York Chocolate manufacturers, built in 1924-30, by architects J G Davies and L E Wade. Red brick in English bond with sandstone ashlar dressings, centrepiece and corner sections, two storeys with roof of north lights surrounded by concrete and ashlar. Baroque Revival style. Façade facing Bishopthorpe Road has centre entrance with panelled double doors and overlight, and distyle in antis porch with Doric columns, all in ashlar. French doors on first floor above with elaborate pedimented triple-key surround and balustraded balcony over the porch. Five windows to either side with six-over-six vertical sashes in architraves, those on ground floor with alternating triangular and segmental pediments and on first floor with cornices. Beyond these, slightly projecting end blocks in sandstone ashlar with quoin strips of alternating bands of brick and ashlar, with central six-over-six sash flanked by narrow four-over-four sashes, of which the central first floor windows have Doric pilasters and triangular pediment with corbelled balustraded balconies. A parapet conceals the roof and is stepped above the end blocks, and in the centre rises to an attic decorated with a festooned cartouche. Left return facing entrance road and right return are identical, with centrepiece between 7 window ranges and end blocks which repeat front façade design. Centrepiece is ashlar with pilasters at the angles and three six-over-six windows on each floor, divided by quoin strips of alternating

brick and sandstone. Parapet above has urns over the quoin strips.

Interior: the plan is of a central double height space surrounded on all four sides by ground and first floor corridors and offices that face the exterior of the building. The original entrance, no longer used, has entrance lobby with inner door leading to broad hallway with tiled floor. Grand staircase rising from centre rear of hall, with splayed lower flight, wrought iron balusters and square wooden newels and handrail, bifurcating from central landing to two flights with quarter turns from centre and higher landings. Wood panelled reception area to right of and partly beneath stair, and corridors off to each side. Central cupola over hallway with coloured glass and ironwork. Ceiling elsewhere is coffered with dentillated cornices and glass panels. Half-height wood-panelled corridors off hallway lead round building, enclosing central large open double height office space, which has glass panelled barrel-vaulted ceiling with dentillated cornices, below roof with north lights. Space partly partitioned with glass and wooden screens, not original. Walls are panelled with pilasters at the corners and semi-circular windows to the first floor at each end. Corridors have original double doors at intervals with decorative glass panels. Offices and other rooms to the outer side of the corridor all have oak panelled doors with eared architraves, and original wooden framed windows. The rooms vary in the amount of architectural detail, the manager's room and the board room having decorated plaster ceilings, classical moulded cornices, and waist-high wainscoting, while other offices are plainer. The

boardroom additionally has a triangular pediment over a plain frieze over the main door, and consoles supporting a cornice above another. First floor rooms are in general of lower status. Some original washbasins and wood screens in lavatories.

HISTORY: Terry's of York began as a confectionary business owned by Bayldon and Berry in 1767 on a site near Bootham Bar. Joseph Terry, connected by marriage to Berry and trained as an apothecary, joined the firm, which moved in 1824 to St. Helen's Square. By 1830, Terry was the sole owner, and gradually developed the chocolate side of the business. New factory premises were built at Clementhorpe in 1862 to provide better transport links, but continued expansion led to the need for new premises which were built at Bishopthorpe Road.

SUMMARY: This building is one of a group consisting of headquarters offices, factory, clocktower, Time Office block and liquor factory, which were built at the same time. The complex is a strong group in architectural terms, presenting a unified style which reflects the strength and importance of the corporate image of Terry's chocolate firm. The buildings also have a strong historic interest, representing the most complete surviving expression of the importance of the confectionary business in York, and confirming, on a national scale, York's high status in this business.

SOURCE: Colbeck, Maurice, 'Made in York' 1992, pp. 23-32.

'Terry's of York 1767-1967' 1967, (Private publication).

'C.M.W.', Journal of Jos. Rowntree, 1925 (pamphlet).



Time Office
Terry's of York

Grade II

Office and transit/recreation room, 1924-30, architects J G Davies and L E Wade for Terry's of York chocolate manufactures. Red brick in English bond with sandstone ashlar dressings, two storeys, flat concrete and asphalt roof. Baroque Revival style.

Façade facing entrance road and headquarters block; central single storey entrance porch with tripartite window above, flanked by quoin strips in alternating red brick and sandstone, with frieze above. Three 2-light windows to either side plus slightly projecting end blocks with tripartite windows similar to centre on both floors. End blocks are framed in ashlar pilaster strips with a frieze over. All windows and frames are C20 replacements. Ashlar faced parapet which is stepped up over centre and blocks. Left return has one 3-light first floor window; right return has two 3-light windows to each floor. Rear has similar fenestration to front, with end blocks defined by quoin strips of alternating brick and ashlar, a door replacing one of the windows to the right of the end block and a central door framed by windows and on overlight.

Interior: the interior is now largely empty and the building is unused. Spiral iron staircase leads to first floor.

The building originally had two archways leading through it from front to back, through which delivery vehicles would pass. The brick infill can be

clearly seen at the rear of the building, showing that the ground floor windows in the ranges to either side of the centre block are later additions. This building was also where employees clocked in, hence its name of the Time Office Block. The building is included for group value.



Factory
Terry's of York

Grade II

Factory built 1924-30 by J G Davies and L E Wade for Terry's of York chocolate manufacturers. Steel framed construction with red brick in English bond with sandstone ashlar dressings and centrepiece, with concrete and ashlar roof. Five storeys, 500 ft. long with entrance front towards central road through site. Central entrance block of ashlar, slightly projecting, flanked by quoin strips of alternating red brick and ashlar sandstone. Double large doors for vehicular access with smaller personal doors to either side separated by Tuscan order columns, with antae to the sides and plain frieze and cornice above. This doorway and a row of small-paned windows above occupy two floors in height. Second and third floors have symmetrically arranged one and two light mullioned and transomed windows with cornices. Vestigial pediment over centre window on second floor. Fourth floor has paired mullion and transom windows in the centre flanked by glazed oculi in keyed ashlar surrounds. To either side of the windows on all floors are mullions and transomed with those on the first floor having cornices and those on the top floor with triple keyed lintels, slightly projecting bay on either side mid way between centre and end blocks. End blocks also project and have mullioned and transomed windows flanked by transomed single window, all with cornices, except top floor which has glazed oculus in keyed surround flanked by transomed

single lights with keyed lintels. Outside projecting end blocks, a tier of single lights in an ashlar strip to full height and alternating quoin strips on corner. Ashlar parapet is stepped above projecting bays, left return towards Bishopthorpe Road has single light tiers in ashlar strip as at front flanking projecting bay with same window arrangement as end blocks to front. Rear is plainer with ranks of metal framed windows with ashlar sills and lintels throughout. At ground level an extension joins the factory to further buildings to the rear which were formally separate.

Interior: Ground floor not fully examined as still in use; entrance leads to hallway with free-standing and engaged pillars. First and remaining floors are now empty. Staircases near each end of building, with toilet facilities on half landings; open spaces within walls half-tiled with white tiles and black banding. Pillars run down the centre length of each floor, and steel frame construction is visible in boxed beams. To the rear of each floor, wooden loading bay doors. Roof houses heating systems etc.

PROCESS: Goods were loaded in at the top floor and manufacturing processes cascaded downwards. The blended beans were roasted, cracked and winnowed, then taken to the nibbing machine where the 'nib' of the bean was extracted, this being the part used for chocolate making. The nibs were ground to produce the cocoa mass, at which point other ingredients such as sugar or milk were added as required. The result was a paste which was refined several times. The next process was 'conching' where the mass was stirred for many

hours at a constant temperature, to produce a smooth cream. In the enrober department the chocolate was added to the various fillings, and the results were then foiled, packaged and dispatched. These processes, once carried out in the factory building, are now more compactly completed in the ground floor of the factory and the 1970 building opposite.



Liquor Factory
Terry's of York

Grade II

Liquor store, now disused, built 1924-30 for Terry's of York by architects J G Davies and L E Wade. Red brick in English bond with sandstone ashlars dressing. Single storey, with raised entrances and a brick parapet concealing a concrete and asphalt flat roof. Frontage to entrance road: central double half-glazed wooden doors with overlights approached up flight of nine steps with brick walls and posts, one side demolished. Former basement windows now bricked up. Mullioned and transomed small pane window to each side. Eaves strip in sandstone below parapet. Left return has two raised entrances up short flights of open stairs, both having overlights. Additional larger entrance to ground level at left end of building. Four windows as at front, basement windows bricked up but two with sandstone lintels remaining. Further buildings added to the rear, not of architectural or special interest. Interior: viewed through windows: now an empty space.



Clock Tower, Water Tower and Boiler house with transformer house
Terry's of York

Grade II

Clock tower, water tower and boiler house, with attached transformer house, built in 1924-30, with later additions, by J G Davies and L E Wade for Terry's of York. Red brick in English bond with sandstone dressings and parapet. Concrete and asphalt roof, metal framed windows. Tower has six stages and attached buildings have three storeys. Tower has brick angle pilasters with sunk-panel ashlar heads at the fifth stage, and large small-paned sash windows with triple keyed lintols. Top stage has ashlar quoins and a clock face on each side on which the letters TERRY YORK replace numbers. The parapet has large corner blocks with balustrades between. The boiler house and transformer house both have small-pane sashes in plain surrounds, and brick parapets above sandstone eaves bonds. Interior: ground floor only: boiler/transformer house still operational with machinery filling the ground floor.

